



701 VALLEY STREET

MULTI-FAMILY RESIDENTIAL BUILDING



PROJECT INFORMATION

PROPERTY ADDRESS

701 Valley Street, Seattle WA

OWNER

701 VALLEY L.L.C.

DEVELOPER

Tarragon L.L.C.
T (206) 233-9600

ARCHITECT

Weinstein A+U L.L.C.
T (206) 443-8606

LANDSCAPE

Thomas Rengstorf & Associates
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PROJECT DESCRIPTION

The proposed project is a 15-story market-rate multi-family residential building approximately 153-feet in height and 145,000-sf. The building will contain 162 residential units and on-site building management and leasing offices. A resident lobby, fitness area, and dog run are located on the first floor, a private outdoor terrace is located on the second floor. Amenity spaces are located at the roof level, including an indoor/outdoor winter garden with operable glass walls and indoor/outdoor kitchen adjacent to an outdoor terrace. Three levels of underground parking accommodate 64 parking spaces. No commercial space is proposed as part of the project.

The project site is 12,163-sf and falls within the South Lake Union Urban Center and the SM-SLU 175/85-280 zone. Significantly, the site is directly under the South Lake Union seaplane flight path, greatly limiting the building’s allowable height. Although a curb cut exists on Aurora Ave., DPD has required that Valley St. be considered the primary off-street parking access.

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ZONING ANALYSIS SUMMARY

LAND USE CODE SUMMARY		
PARCEL NO	2249000265	
LOT AREA	12,163-sf (0.28 acres)	
ZONING	SM-SLU 175/85-280, South Lake Union Urban Center, South Lake Union Seaplane Flight Path	
PERMITTED USES	<ul style="list-style-type: none">Residential uses: Permitted Outright	Will comply
STREET LEVEL STANDARDS	<ul style="list-style-type: none">Minimum height for street facing facades: 15'Blank facade segments to be ≤30 feet and limited to no more than 70% of the facade width or 78% if street frontage slope exceeds 7.5%	Will comply Valley Street as a 10.5% slope
STRUCTURE HEIGHT	<ul style="list-style-type: none">Maximum structure height as zoned: 250'SLU Urban Center Flight Path limits height per map A23.48.010<ul style="list-style-type: none">Rooftop features and mechanical equipment must be under height limit	Will comply
FLOOR AREA RATIO	<ul style="list-style-type: none">FAR for all residential uses is exemptUnderground all residential uses exemptGross floor area:<ul style="list-style-type: none">Upper-level floor area limited to 50% of lot size unless development is prevented from reaching maximum height by Flight PathLots less than 12,500 sq-ft shall have an increase in upper-level floor area from 50% to 67%Podium:<ul style="list-style-type: none">Podium height is shown on Map A for 23.48.245 and is measured from grade elevation at street lot line<ul style="list-style-type: none">Podium height is 65' per Map AFor podiums of residential uses the average floor area coverage shall not exceed 75% of lot area	Will comply
		Will comply
SETBACKS	<ul style="list-style-type: none">No upper level setbacks requiredNo facade modulation required for residential uses	
LANDSCAPING STANDARDS	<ul style="list-style-type: none">Green Factor of ≥0.30 required	Will comply
AMENITY	<ul style="list-style-type: none">Residential amenity area equal to 5% of residential gross floor area and meet the following:<ul style="list-style-type: none">All residents have access to the required amenity areaAmenity areas limited to 50% enclosedCommon residential amenity areas shall have a dimension of at least 15-ft and a minimum area of 225-sfAmenity areas that are provided as landscaping at street level open space shall be counted as twice the actual area	Will comply
PARKING	<ul style="list-style-type: none">Parking for residential uses<ul style="list-style-type: none">No parking required within urban centersBicycle Parking<ul style="list-style-type: none">Multi-family structures, 1/1 DU long-term, 1/20 DU short-term	Will comply

CONTEXT ANALYSIS



Neighboring Buildings

- Four-story 701 Dexter Avenue N office building to the south
- Six-story Hue Apartments (717 Dexter Avenue N) located immediately to the east
- Six-story Stream Uptown Apartments (708 6th Avenue N) located across Aurora Avenue N
- Seven-story Juxt Apartments (810 Dexter Ave N) located across the intersection of Dexter Ave and Valley Street to the northeast
- Fourteen-story Dexter Yard mixed-use office building directly across Dexter Ave to the east is currently under construction
- Aurora Avenue N auto-dominated with few buildings addressing the street in a meaningful way

Street Traffic

- Aurora Avenue N (SR 99) is a regional highway serving as a north-south connection for the Seattle metropolitan area. Bus routes with limited stops use the Aurora corridor (MT #5, #26, #28 & BRT E Line)
- Dexter Avenue N is a minor automobile arterial and principal bicycle route. One bus route (MT #62 serves the corridor
- Valley Street is a one-way street (westbound) serving primarily to access northbound SR 99

Streetscape

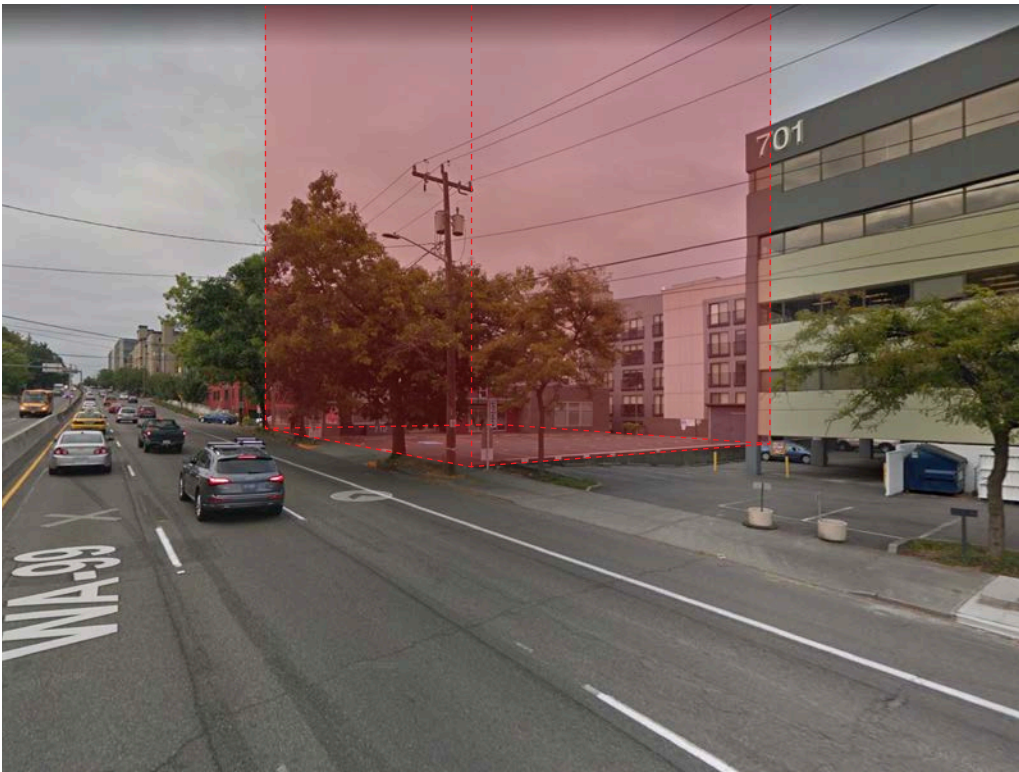
- Pedestrian and bicycle improvements along the Dexter corridor have been made in conjunction with the recent widespread redevelopment of the area. Pedestrian improvements such as new sidewalks, planting beds and street trees have been added to Valley Street adjoining newly constructed buildings
- Frontage along Aurora Avenue N characterized by root bound street trees, heaved sidewalks and curbs, and broken pavement

- (N) Building Footprint
- Bus Routes
- Bike Lanes
- Zoning Boundaries
- (N) Building Entry/Exit

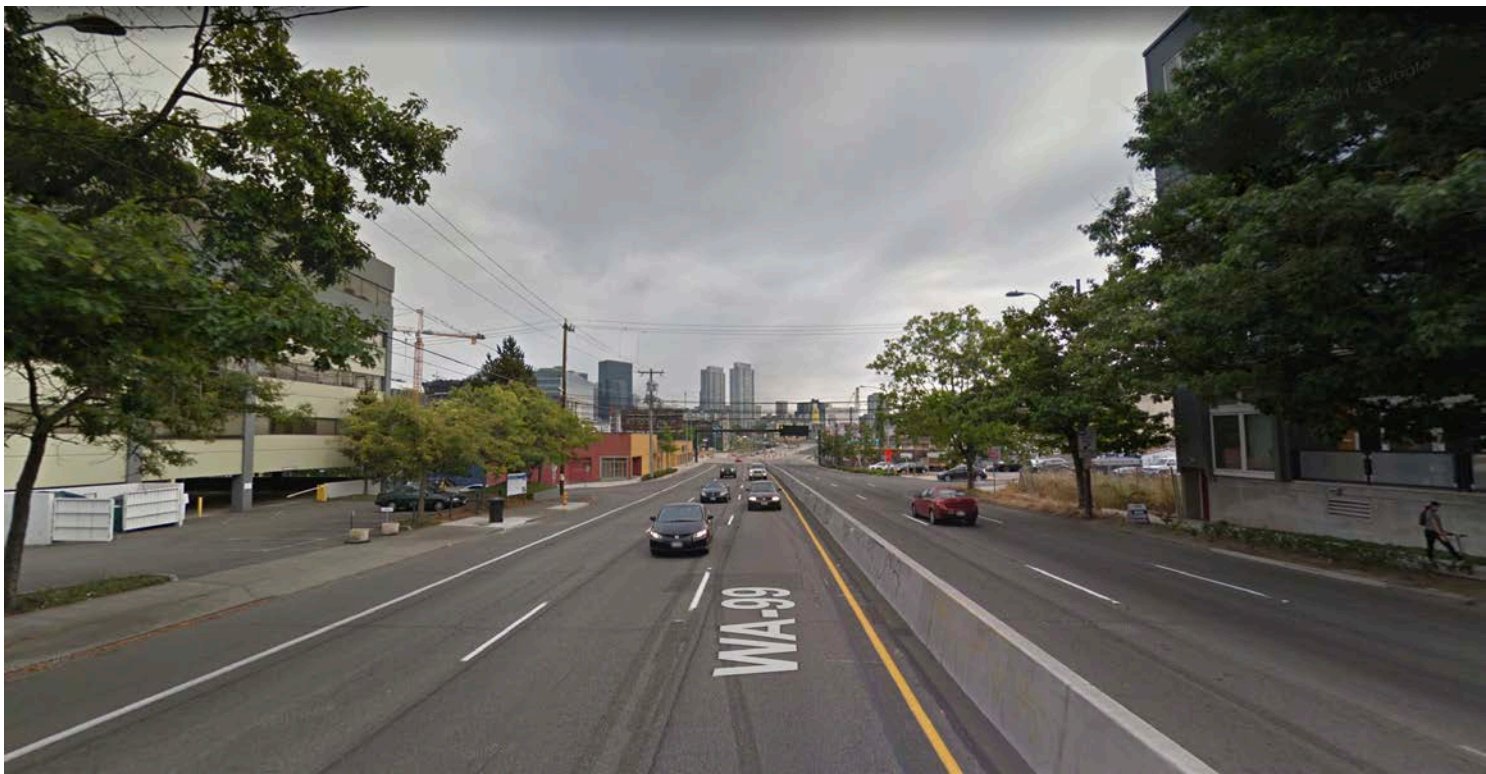
NEIGHBORHOOD CONTEXT



① View North on Aurora Avenue



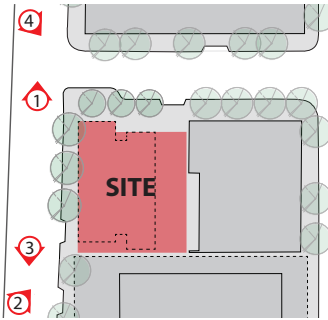
② View of Site Looking NE (Zoning Potential Highlighted in Red)



③ View South on Aurora Avenue

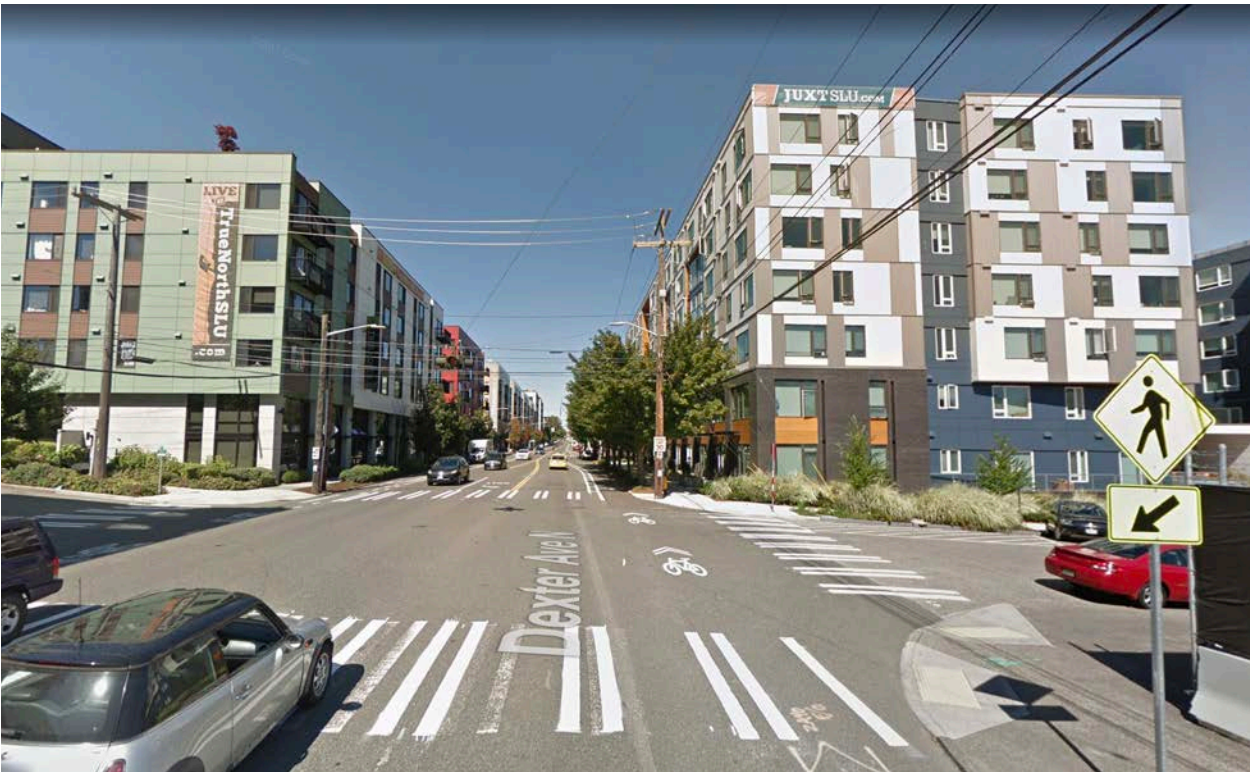


④ View of Site Looking SE (Zoning Potential Highlighted in Red)

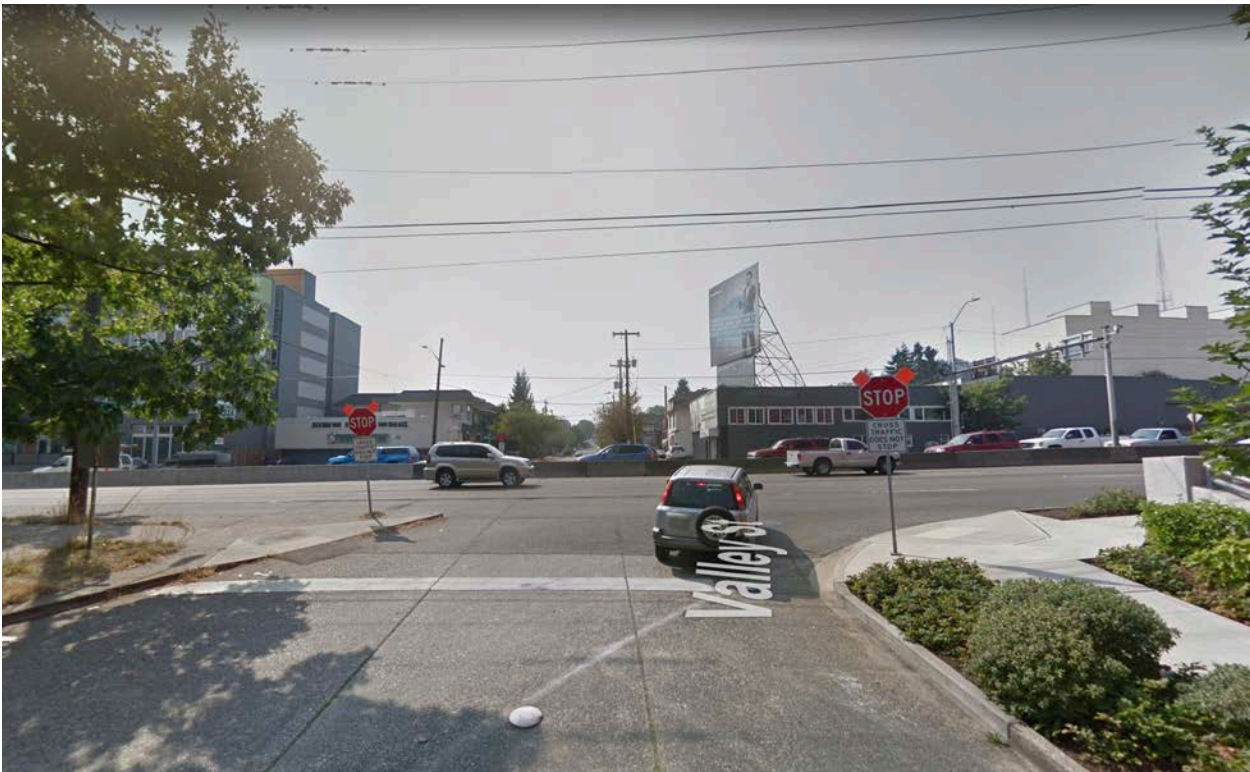




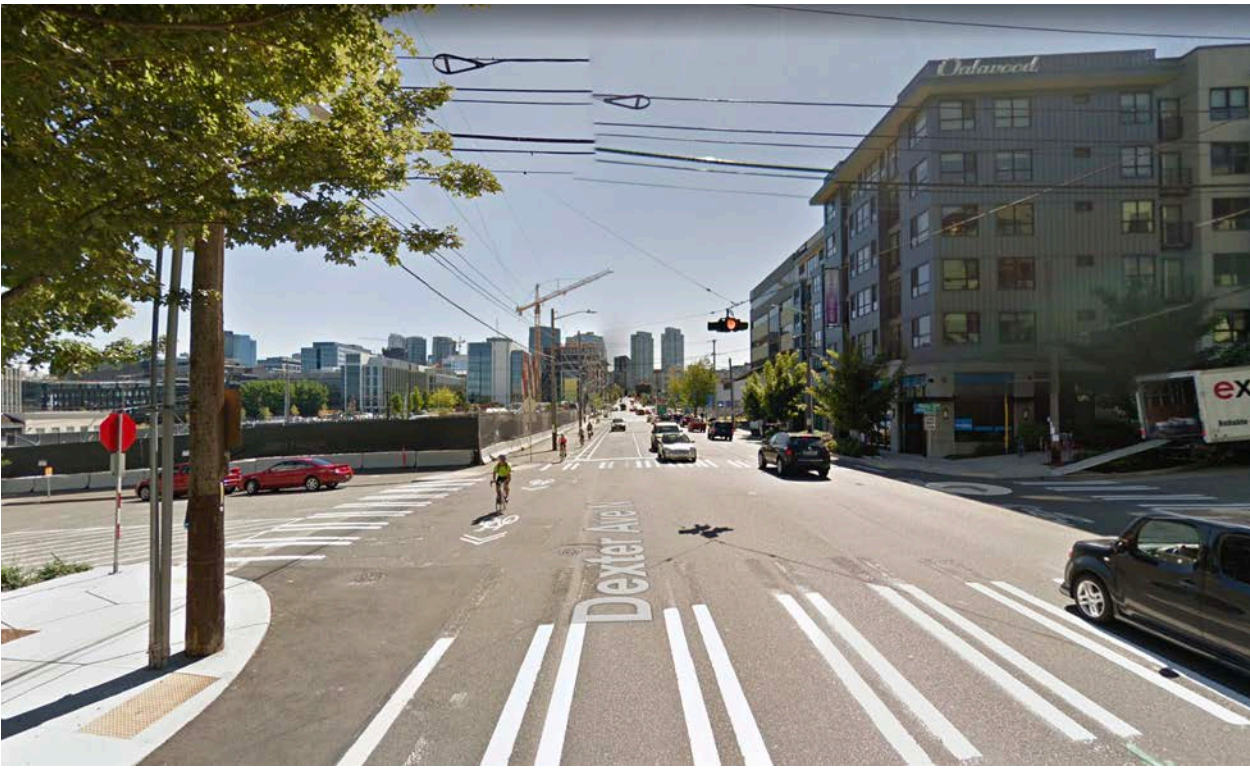
① View East on Valley Street



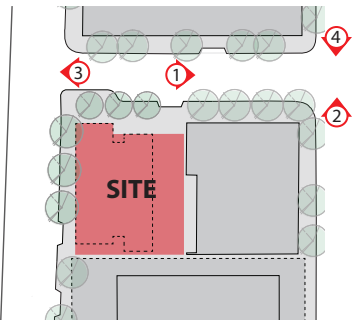
② View North on Dexter Avenue



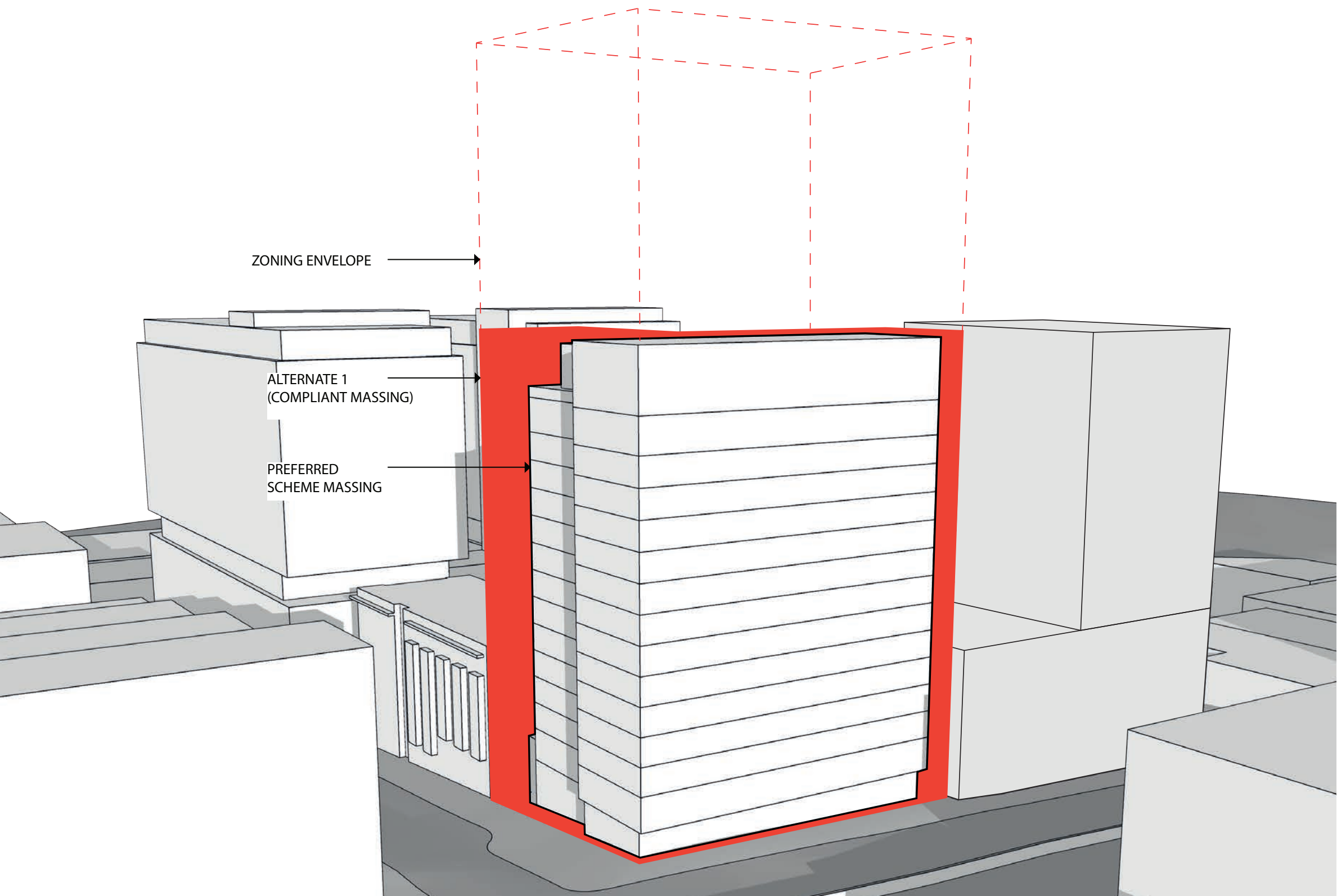
③ View West on Valley Street



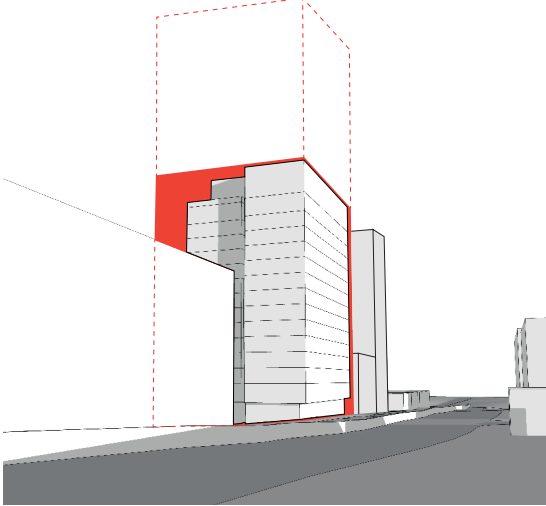
④ View South on Dexter Avenue



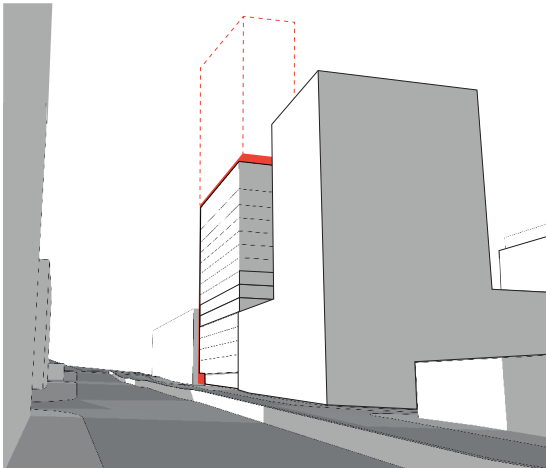
MASSING: OPEN SPACE & ADJACENCIES



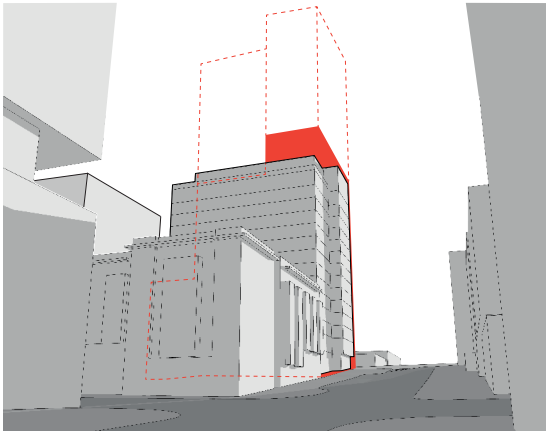
View southeast from Queen Anne Hill



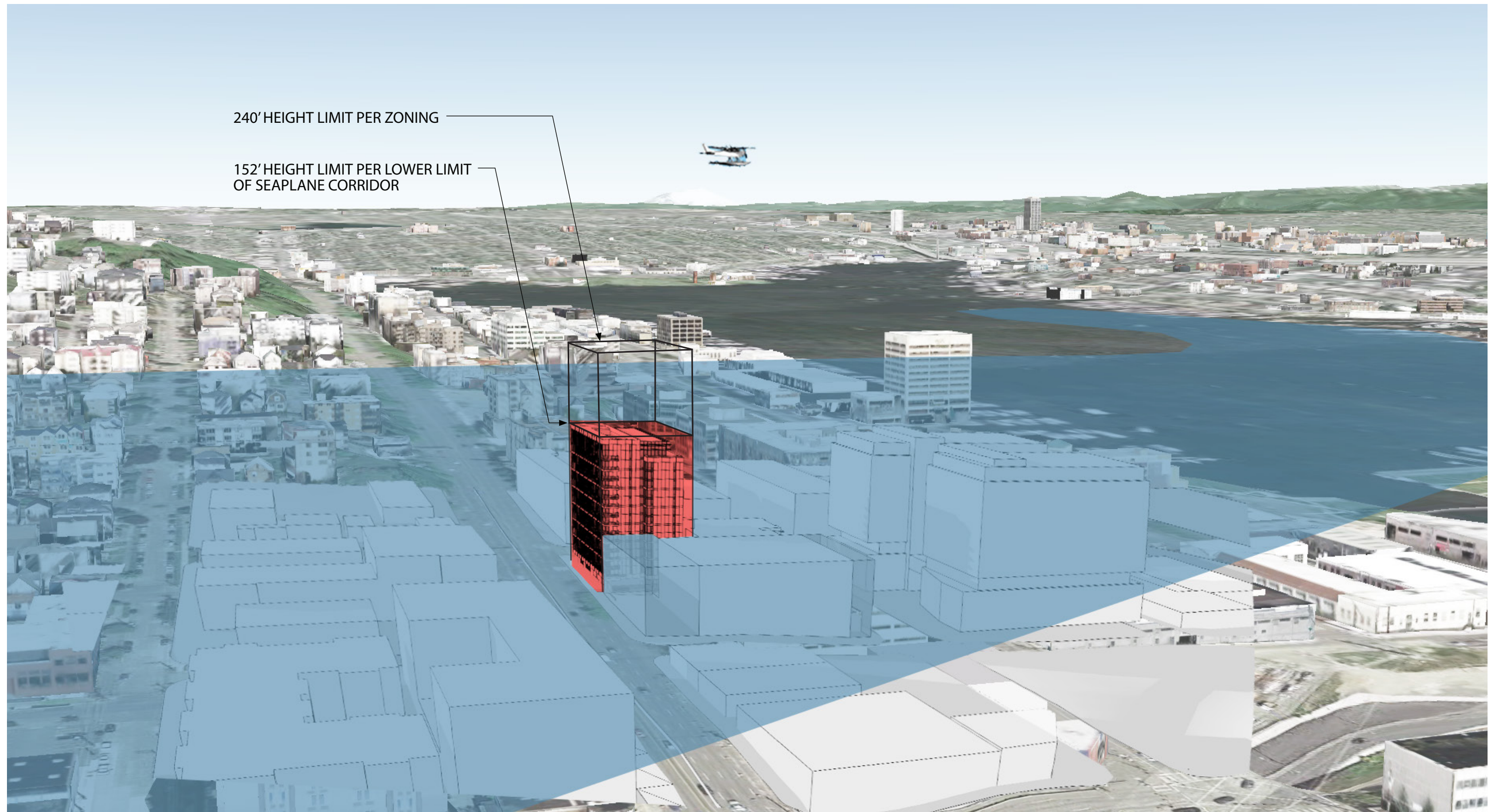
View southeast on Aurora Avenue



View northeast on Aurora Avenue



View west on Valley Street



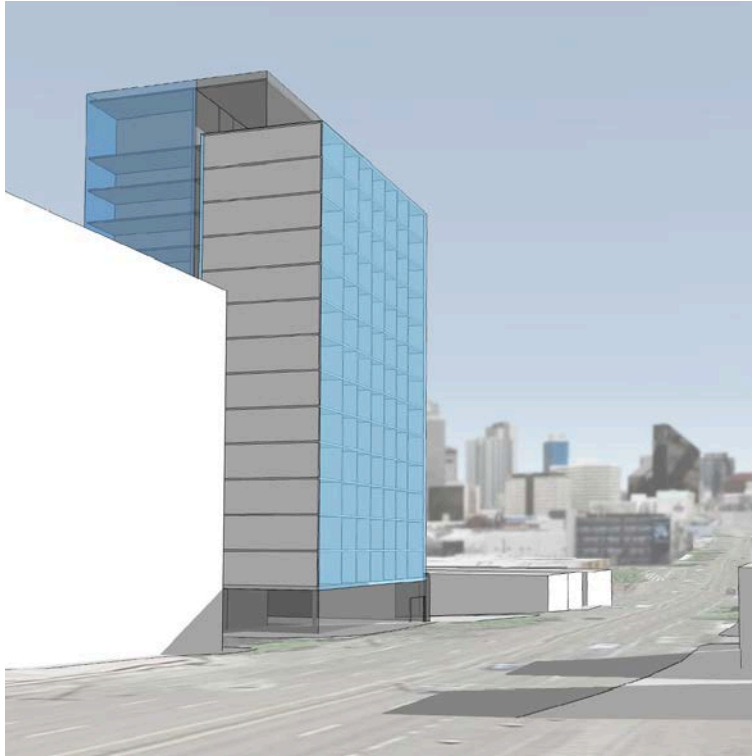
APPROVED EARLY DESIGN GUIDANCE CONCEPT

KEY DESIGN FEATURES:

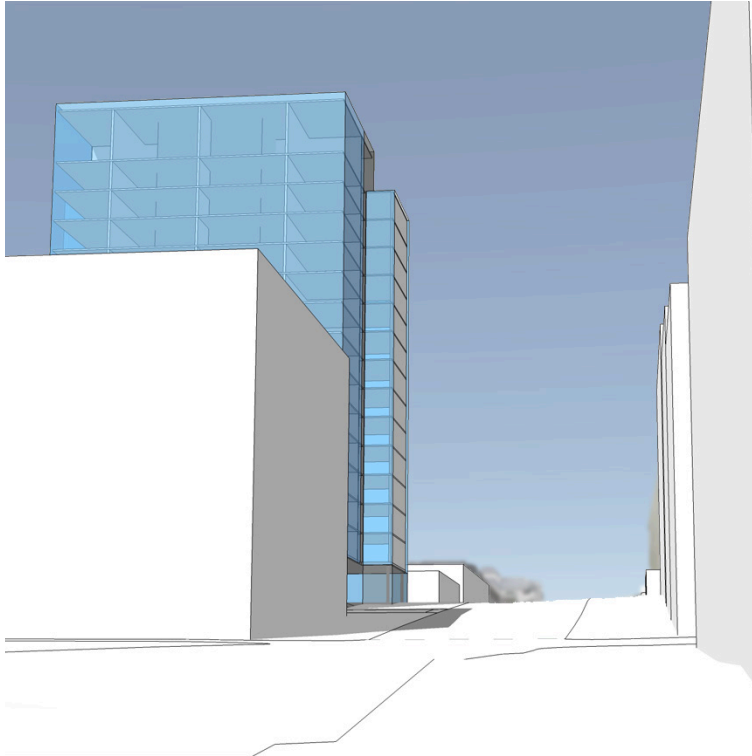
- Pedestrian and garage entry on Valley Street
- Shared amenity spaces at level 1 and 15
- Reduced impact to urban views and sunlight access
- Simplified building massing stepping back from property line creates a narrow building profile
- E/W oriented units provide equitable access to views
- Maximize glazing at E/W facades



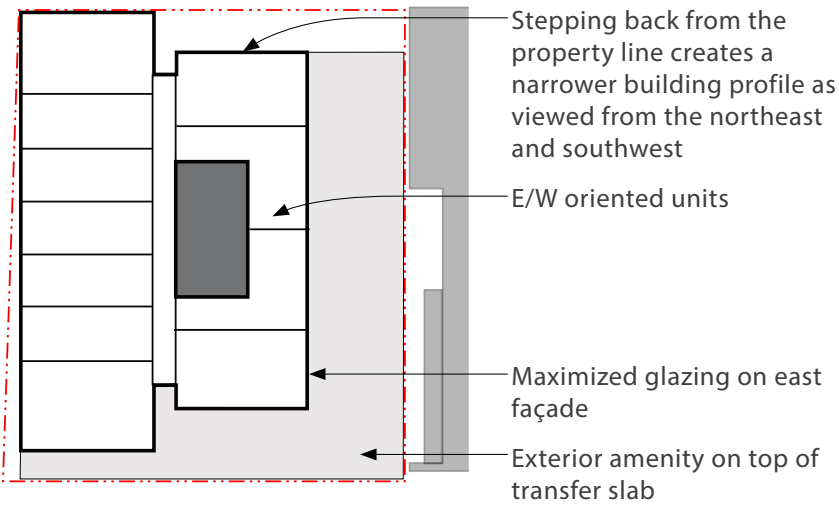
Looking northeast on Aurora Ave N



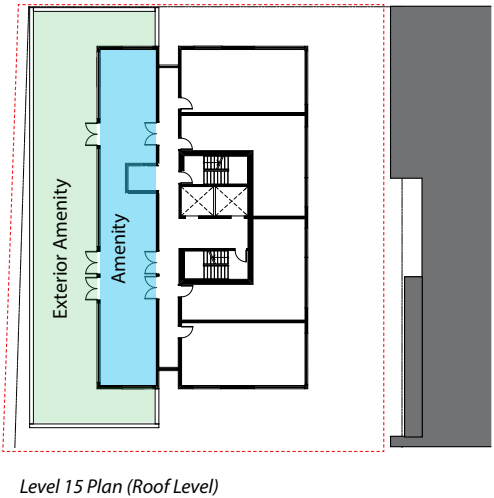
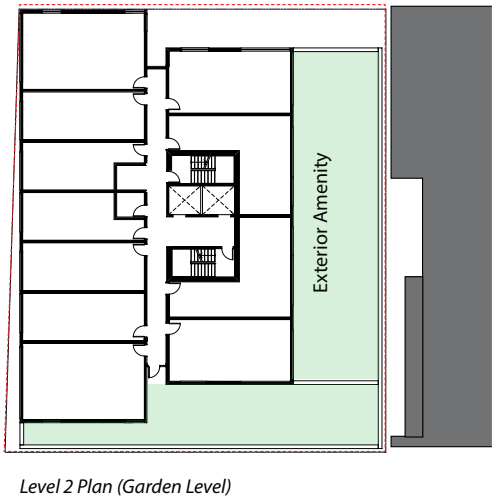
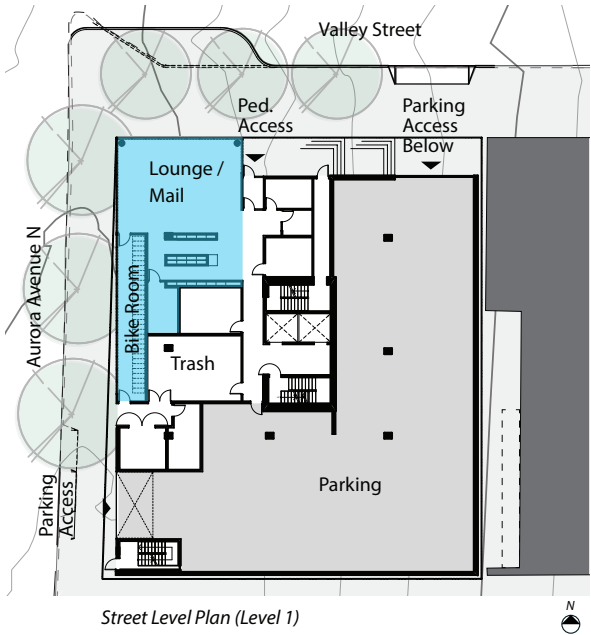
Looking southeast on Aurora Ave N



Looking west on Valley Street, towards building entry



Tower Plan Diagram

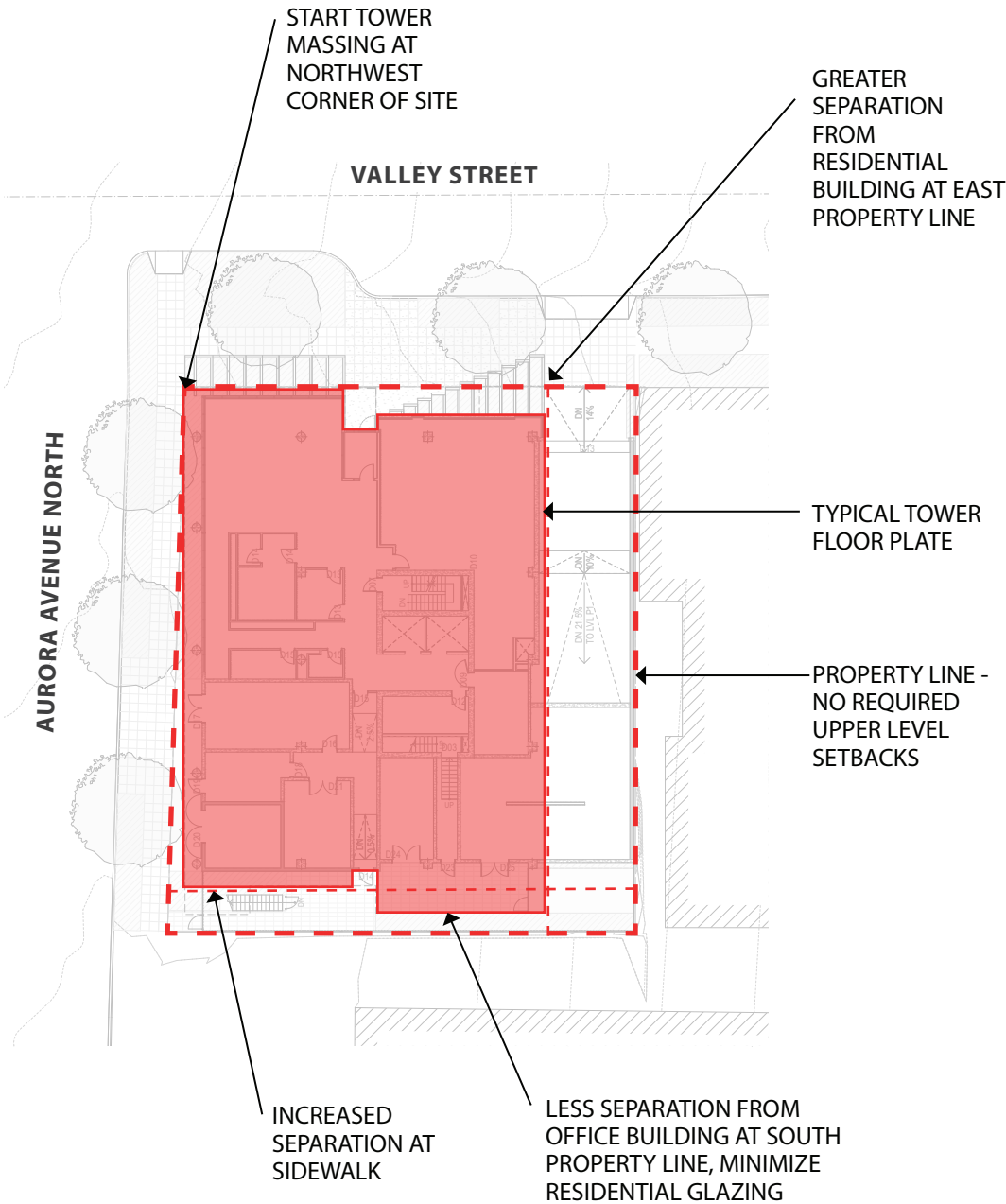




Looking southwest on Aurora Ave N



Looking west on Valley Street



Plan diagram showing tower location

1 MASSING
The Board directed the applicant to proceed with the preferred option as it has a better relationship to the surrounding structures and provides better open space for residents. The Board supported the orientation of the residential units facing west and east, and also pulling the massing away from the neighboring existing structures. The Board recommended the design team locate the structure to set a precedent for orientation for views and the treatment of Aurora Ave N.

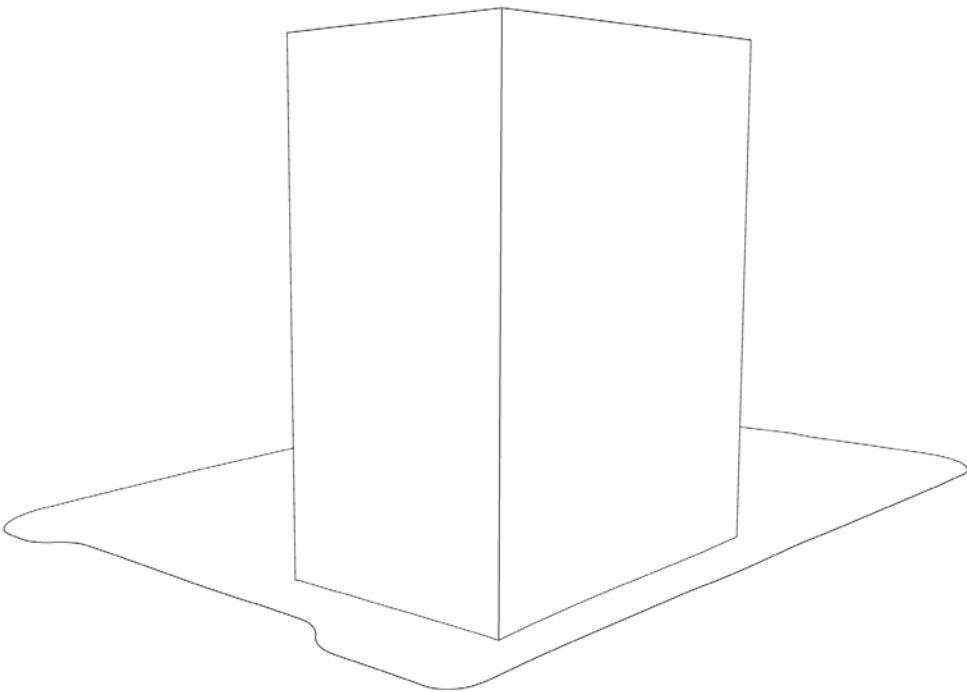
Response: The slender tower and shift in plan minimizes the profile of the building as seen from from Aurora Ave N and Queen Anne Hill. Additionally, a step in the building’s massing at the rooftop creates an east-oriented roof terrace. Because of building code requirements limiting the opacity of walls proximate to property lines, and the energy code’s limits on glazing, the design strategy for the residential slabs is to maximize glazing on the east and west elevations, while treating the relatively short, north and south elevations as mostly opaque with limited punched openings. While the pedestrian entrance is oriented to Valley Street, the building uses its massing to addresses Aurora Avenue, its most visible exposure, with a strong edge, and an identifiable, activated lobby use at the street level. With its stepped and shifted residential slabs, the building establishes a neighborhood precedent for urban architecture that responds to both topography and the dynamic flight path overlay, while maintaining clear expressions of its program

RESPONSE TO EARLY DESIGN GUIDANCE

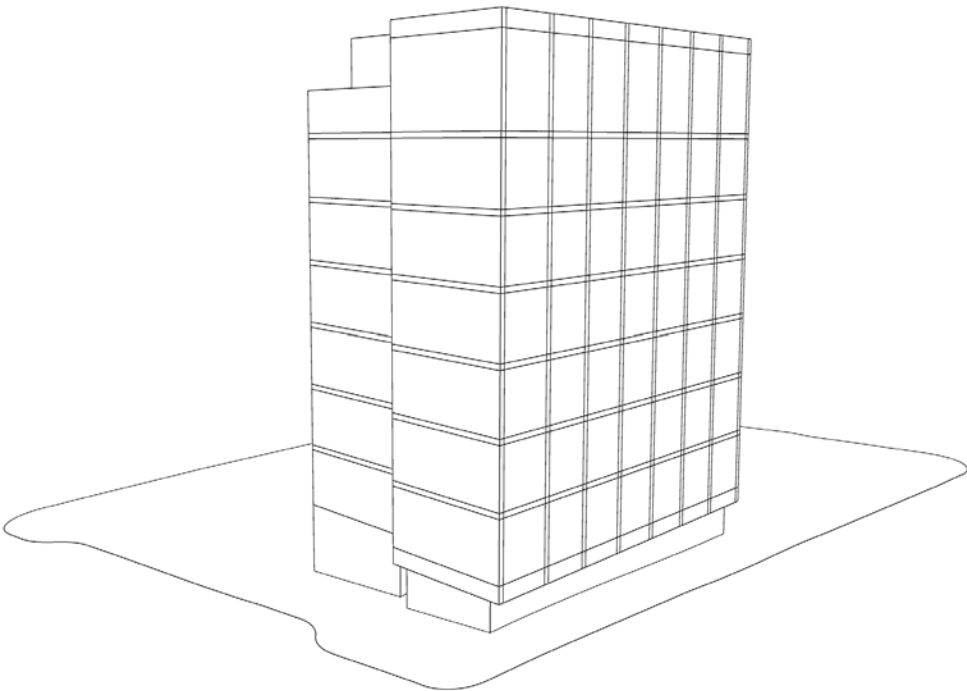
2 TOWER DESIGN

The Board noted the top of the structure will be visible, and expressed it was up to the design team to decide if the east or west location for the amenity and open space on the roof is the best location. The Board supported the modulation on the north and south elevations, and requested that the west elevation use secondary architectural features to create a finer human scale detailing. The Board recommended designing the tower so it has a residential appearance and character.

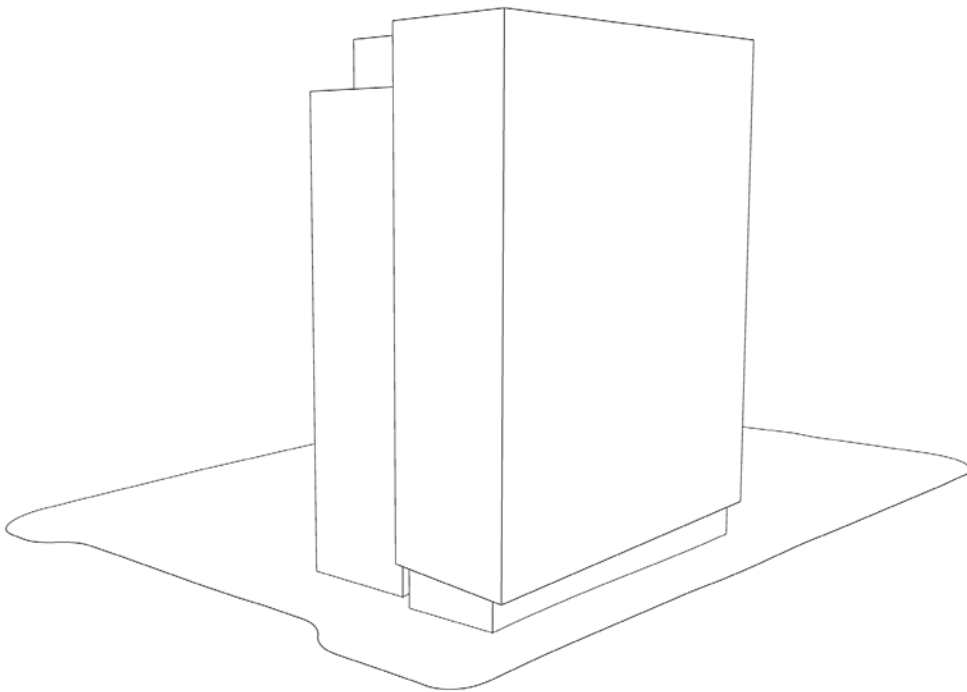
Response: The building is divided into masses that shift in plan and step in elevation in direct response to the sites topography and urban context. The residential units are contained in slender volumes organized about a corridor. Balancing views with the desire for privacy, the volumnas are expressed as a combination of opaque panels and generous windows. Opaque walls are concentrated at the north and south elevations, providing generous floor to ceiling view windows in all units. Common spaces, such as the corridors, residential lobby, and rooftop amenity area are recessed, contiguous, and expressed with a contrasting material palatte of mostly glass and aluminum. The scale of the residences, and the cellular nature of grouping the opaque elements will provide a variety of scales and textures to increase the visual interest of the façades and will express a scale understood to reflect residential design. The different unit layouts on east and west orientations, the proportions, and the presence of the rooftop amenity area will help to differentiate the east façade as a secondary expression relative to the more prominent west façade.



Locate massing at NW corner of site to maximize separation from adjacent propertiees



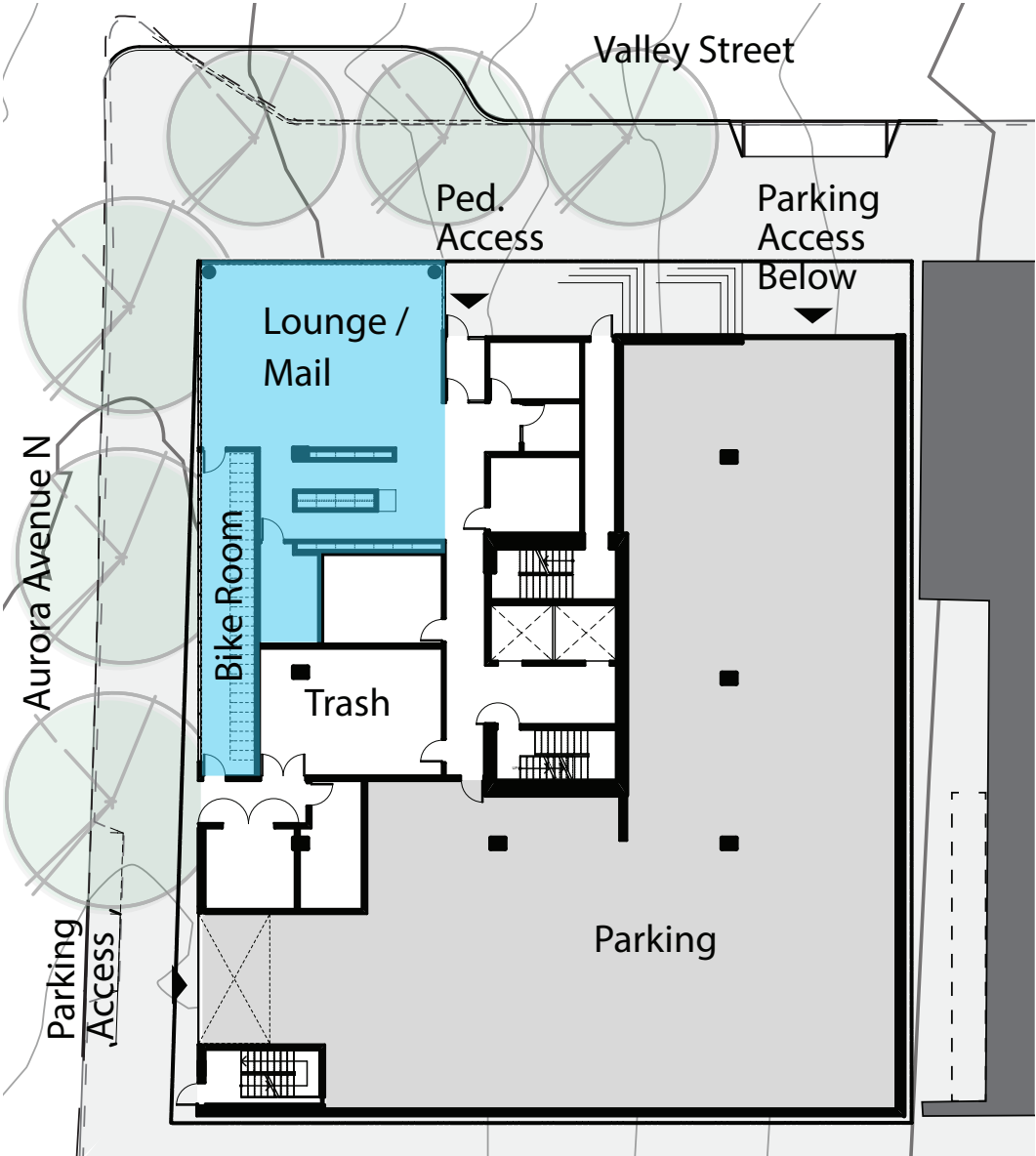
Apply organizational grid to break down scale of building and create hierarchy of architectural expression



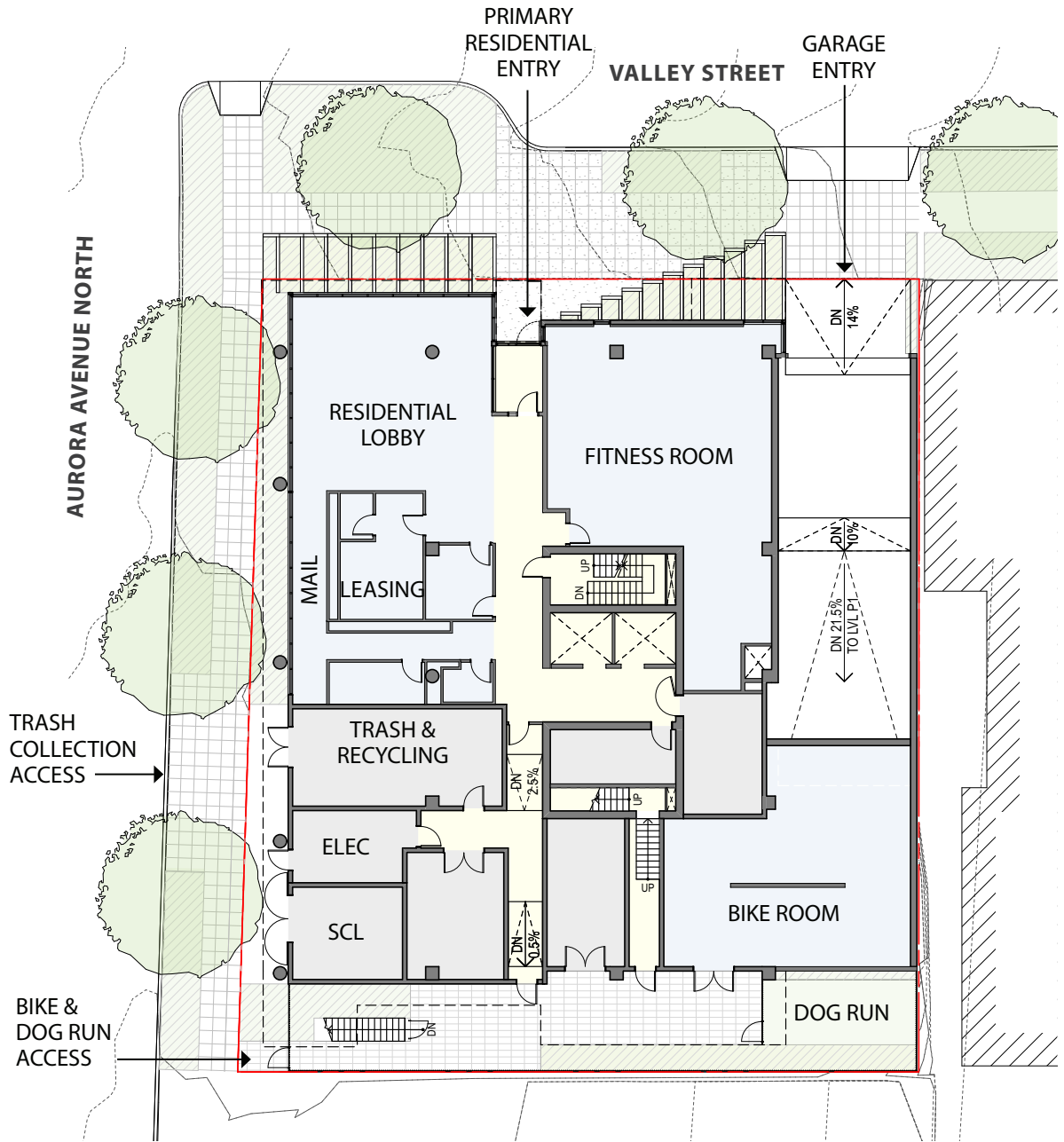
Shift and step massing to respond to site topography and context, reducing the profile of the building



Apply panel joints and exterior balconies, add staggered mullions and operable windows to emphasize scale and cellular organization of residential units



EARLY DESIGN GUIDANCE STREET LEVEL PLAN



UPDATED STREET LEVEL PLAN

3 RELATIONSHIP TO STREET
The Board encouraged the location of the entry lobby and the cascading stairs towards Dexter Ave N. The Board recommendation prioritizing the street level for residents to enter and exit the site safely. The Board supported the placement of bike storage along Aurora Ave N, and requested careful consideration of how the street edge will work.

Response: The primary pedestrian entry is located on Valley Street, clearly marked by the building's massing. Given that most pedestrians arriving to the side will be coming uphill from Dexter Avenue to the east, this lobby entrance is both convenient and visible. The stepped landscaped planters also serve to guide the pedestrians to the entrance. A pedestrian access gate at the southwest corner of the site along Aurora Ave provides access to the rear courtyard, dog run, garage stairs, and bicycle storage room.

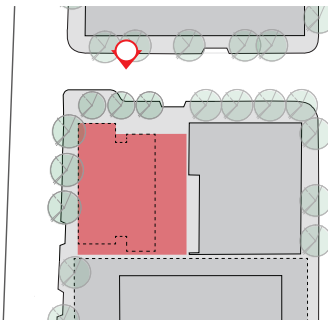
4 ACCESS
The Board discussed the departure request to allow a second curb cut off of Aurora Ave N, and noted that they would not be inclined to grant this departure. The Board acknowledged that a curb cut for solid waste pickup only may facilitate pullover space and access for collection trucks. However, they expressed that the solution to this should be determined by SPU, SDOT and DPD.

Response: Parking garage access is provided from Valley Street at the north of the site. Access to the solid waste pick-up is located on Aurora Ave at the southern end of the site to provide separation from the entry lobby. Solid waste pick-up access off of Aurora and associated curb modification has been reviewed and supported by SPU. The design team is no longer requesting a departure for a curb cut off of Aurora.

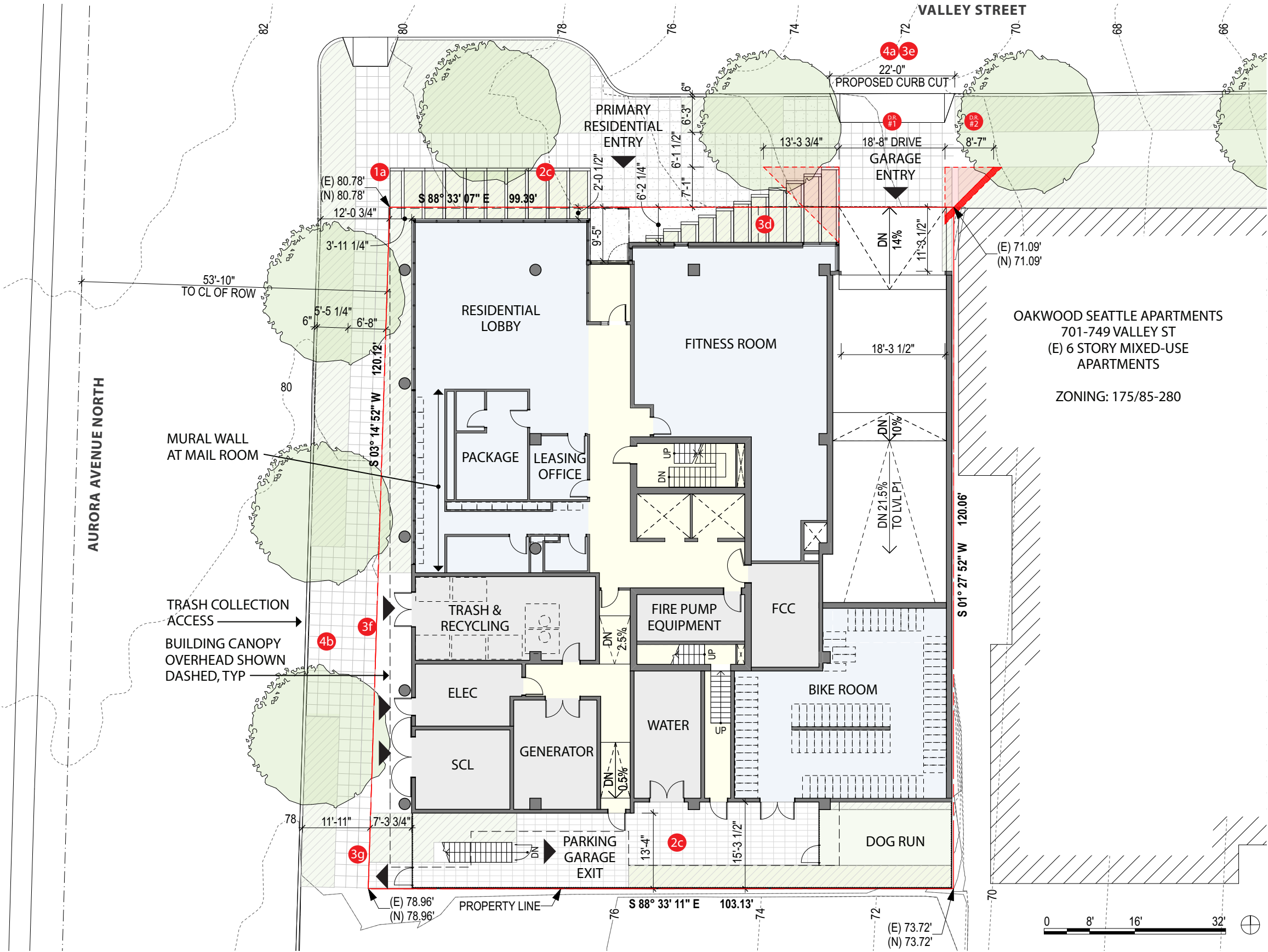
STREET LEVEL RESIDENTIAL ENTRY OFF VALLEY ST

The bullet points below and on the following pages summarize the guidance provided to the design team by the Board at the Early Design Guidance meeting on January 7, 2015. The guidance has been grouped into categories that appear on the right side of the following two-page spreads with a brief description of how the design has been developed in response to the Board's guidance. The red bullets correspond to board priorities and recommendations, and the black bullets correspond to design review guidelines listed as project priorities

- CS B2 Connection to the Street
- CS D5 Respect for Adjacent Sites
- PL A2 Access Challenges
- PL B3 Street-Level Transparency
- PL A1 Design Objectives
- PL A1c Common Entries to Multi-Story Residential Buildings
- PL A4 Ensemble of Elements
- PL A1 Serving All Modes of Travel
- DC B1 Access Location and Design
- DC D2 Texture



STREET LEVEL PLAN



- CS B2 Connection to the Street
- CS D5 Respect for Adjacent Sites
- PL A2 Access Challenges
- PL B3 Street-Level Transparency
- PL A1c Common Entries to Multi-Story Residential Buildings
- PL A4 Ensemble of Elements
- PL A1 Serving All Modes of Travel
- DC B1 Access Location and Design

Massing

1a Tower is positioned at the NW corner of the site

Tower Design

2c Opaque walls are concentrated at the N and S

2e Common spaces are recessed, contiguous, and expressed with a complimentary materiality

Relationship to Street

3a A full height reveal marks the primary entry

3b The lobby level is a continuous horizontal gasket

3c Residential lobby is visible from Dexter Avenue

3d Planters step up the hill and shift towards the entry

3e Curbs, planting, and pavement texture increase car awareness of a pedestrian zone

3f The SCL vault and solid waste pickup on Aurora Ave creates separation from pedestrians to the north

3g Access to the rear courtyard is through a secure gate

Access

4a Parking access is from Valley Street

4b Solid waste pickup utilizes the existing curb cut on Aurora

DR #1 Development Standard Departure Request #1, pg 40

DR #2 Development Standard Departure Request #2, pg 41

ROOF TERRACE AMENITY SPACE



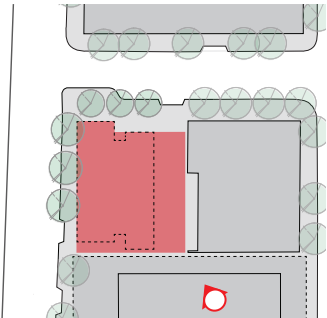
- CS B1 Site Characteristics
- CS D2 Existing Site Feature
- CS D5 Respect for Adjacent Sites
- DC A1 Site Characteristics and Uses
- DC A2 Reducing Perceived Massing
- DC B1 Facade Composition

Massing

- 1e East-oriented roof terrace with views to Lake Union
- 1f Stepped and shifted slabs respond to topography and the dynamic flight path overlay

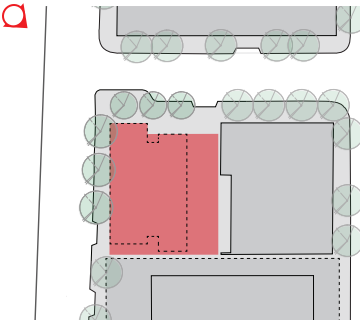
Tower Design

- 2c Opaque walls are concentrated at the N and S
- 2e Common spaces are recessed, contiguous, and expressed with a complimentary materiality
- 2f Massing of the tower creates two slightly unique expressions envisioned as part of a whole



VIEW FROM AURORA AVE N

- CS B2 Connection to the Street
- CS A4 Evolving Neighborhoods
- PL2 A2 Access Challenges
- PL2 B3 Street Level Transparency
- PL3 A1c Common Entries to Multi-Story Residential Buildings



WEST ELEVATION (AURORA AVENUE NORTH)



- CS B2 Connection to the Street
- CS A4 Evolving Neighborhoods
- PL A2 Access Challenges
- PL B3 Street Level Transparency
- PL A1c Common Entries to Multi-Story Residential Buildings

Massing

- 1a** Tower is positioned at the NW corner of the site
- 1b** The primary building frontage engages Aurora Ave

Tower Design

- 2b** East and west facades are expressed as alternating groups of opaque panels and generous windows
- 2d** A cellular grouping of opaque elements creates a unique visual scale

Relationship to Street

- 3b** The lobby level is a continuous horizontal gasket
- 3f** The SCL vault and solid waste pickup on Aurora Ave creates separation from pedestrians to the north
- 3g** Access to the rear courtyard is through a secure gate

Access

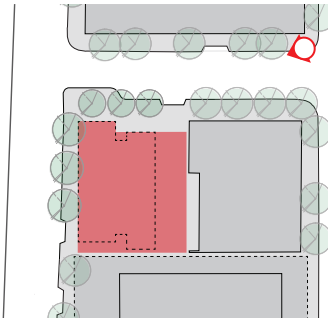
- 4b** Solid waste pickup utilizes the existing curb cut on Aurora

Materials Key:

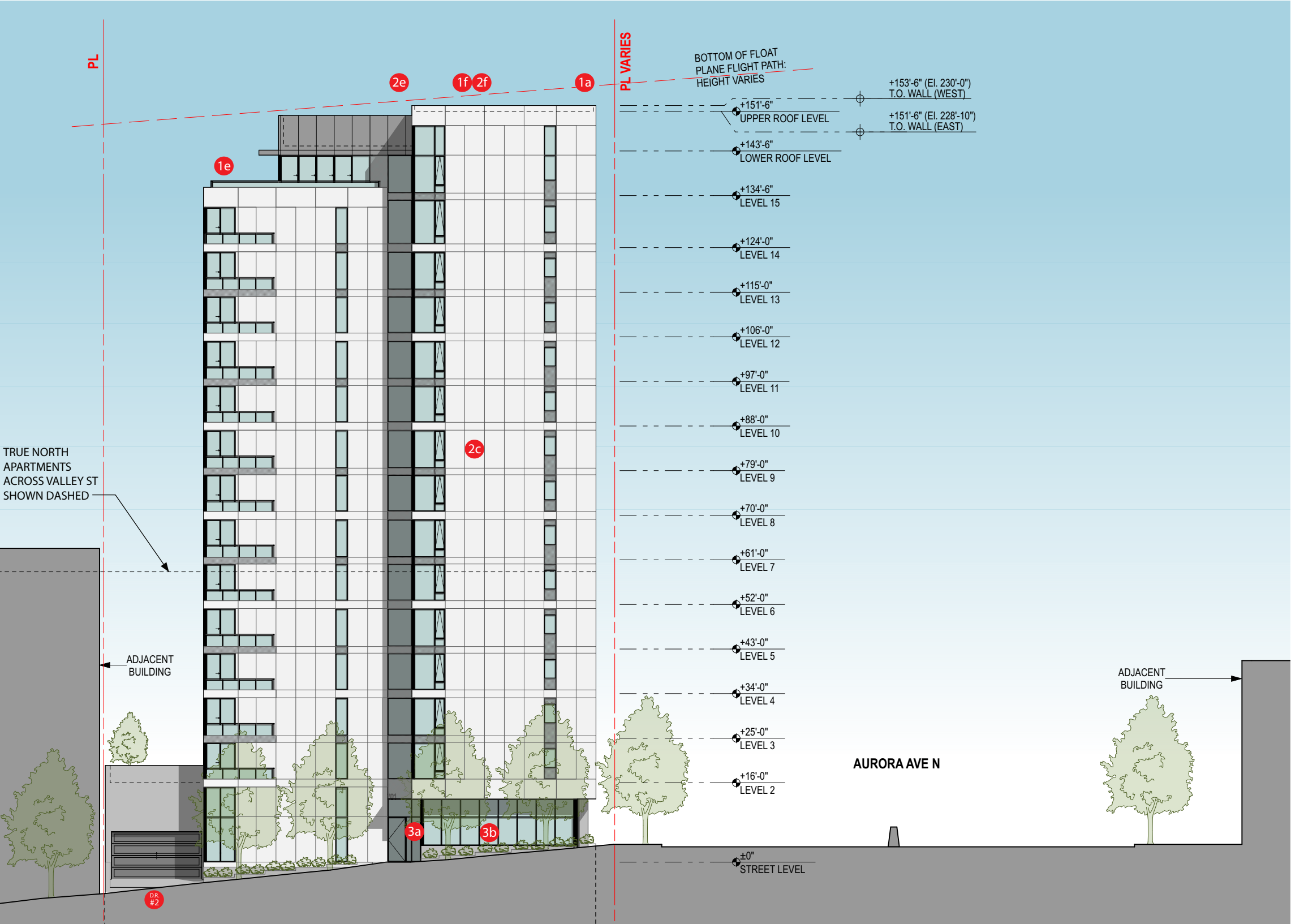
- Vision Glass
- Spandrel Glass
- Fiber Cement Panel 1
- Fiber Cement Panel 2 (Not Shown)
- Metal Panel
- Metal Louvre
- Aluminum Window Wall System
- Concrete

VIEW FROM VALLEY ST

- CS2 B1 Site Characteristics
- CS2 B2 Connection to the Street
- CS2 D5 Respect for Adjacent Sites
- PL2 A2 Access Challenges
- PL2 B3 Street Level Transparency
- PL3 A1 Design Objectives
- PL3 A4 Ensemble of Elements
- PL4 A1 Serving all Modes of Travel



NORTH ELEVATION (VALLEY STREET)



- CS B2 Connection to the Street
- CS D5 Respect for Adjacent Sites
- PL A2 Access Challenges
- PL B3 Street Level Transparency
- PL A4 Ensemble of Elements
- PL A1 Serving all Modes of Travel

Massing

- 1a Tower is positioned at the NW corner of the site
- 1e East-oriented roof terrace with views to Lake Union
- 1f Stepped and shifted slabs respond to topography and the dynamic flight path overlay

Tower Design

- 2c Opaque walls are concentrated at the N and S
- 2e Common spaces are recessed, contiguous, and expressed with a complimentary materiality

Relationship to Street

- 3a A full height reveal marks the primary entry
Residential lobby is visible from Dexter Avenue
- 3b The lobby level is a continuous horizontal gasket
- 3e Curbs, planting, and pavement texture increase car awareness of a pedestrian zone

Access

- 4a Parking access is from Valley Street

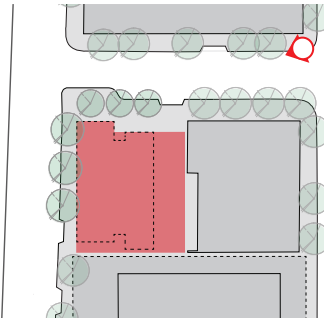
- DS #2 Development Standard Departure Request #2, pg 41

Materials Key:

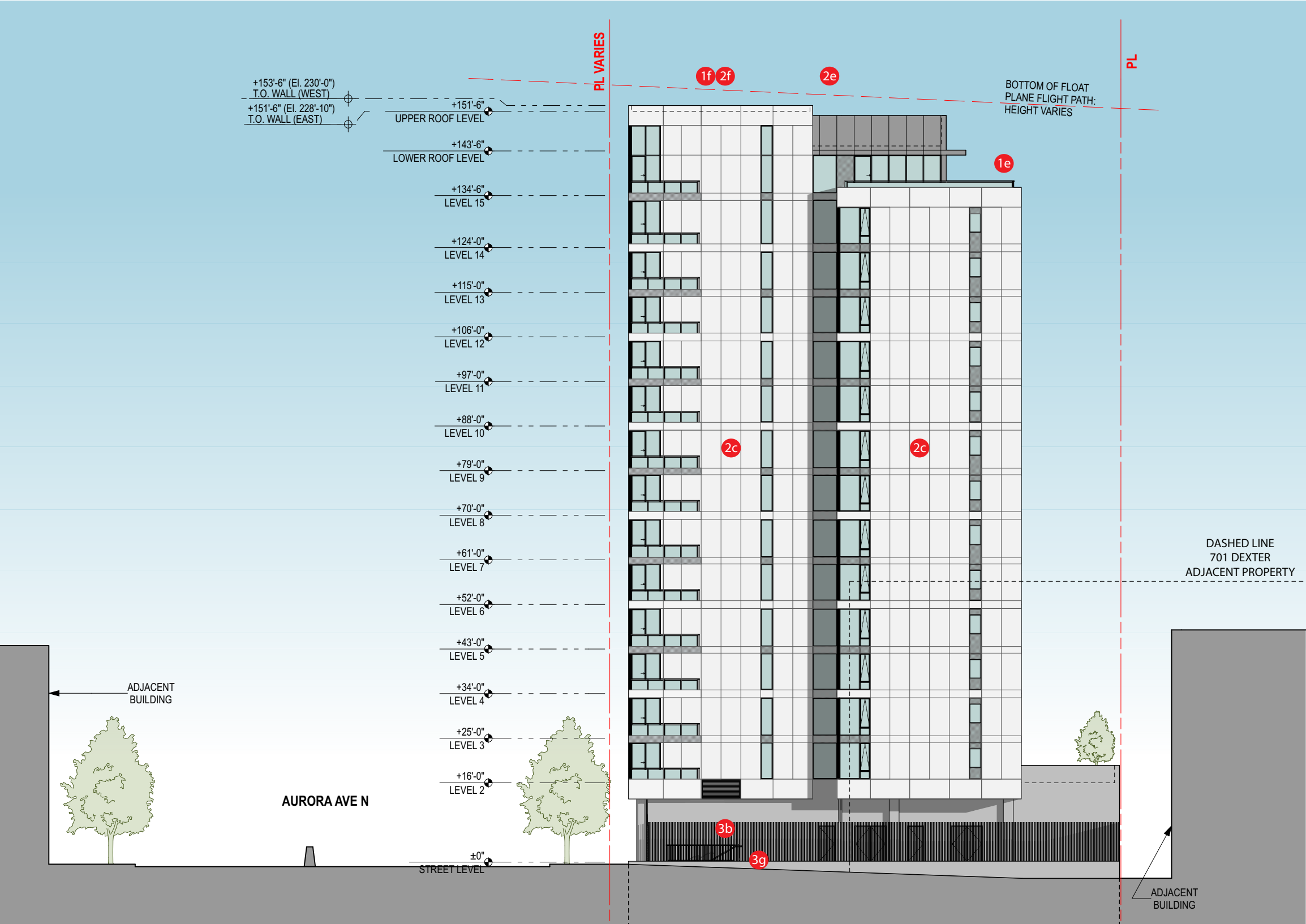
- Vision Glass
- Spandrel Glass
- Fiber Cement Panel 1
- Fiber Cement Panel 2
- Metal Panel (Not Shown)
- Metal Louvre (Not Shown)
- Aluminum Window Wall System
- Concrete

VIEW FROM AURORA AVE N

- CS1 B1 Site Characteristics
- CS2 B2 Connection to the Street
- CS3 D5 Respect for Adjacent Sites
- PL2 A2 Access Challenges
- PL2 B3 Street Level Transparency
- PL3 A1 Design Objectives
- PL3 A4 Ensemble of Elements
- PL4 A1 Serving all Modes of Travel



SOUTH ELEVATION (PROPERTY LINE)



- CS A2 Architectural Presence
- CS B1 Site Characteristics
- CS D5 Respect for Adjacent Sites
- PL A1 Serving all Modes of Travel
- DC A1 Site Characteristics and Uses
- DC B1 Facade Characteristics
- DC B2 Blank Walls

Massing

- 1e East-oriented roof terrace with views to Lake Union
- 1f Stepped and shifted slabs respond to topography and the dynamic flight path overlay

Tower Design

- 2c Opaque walls are concentrated at the N and S
- 2e Common spaces are recessed, contiguous, and expressed with a complimentary materiality
- 2f Massing of the tower creates two slightly unique expressions envisioned as part of a whole

Relationship to Street

- 3b The lobby level is a continuous horizontal gasket
- 3g Access to the rear courtyard is through a secure gate

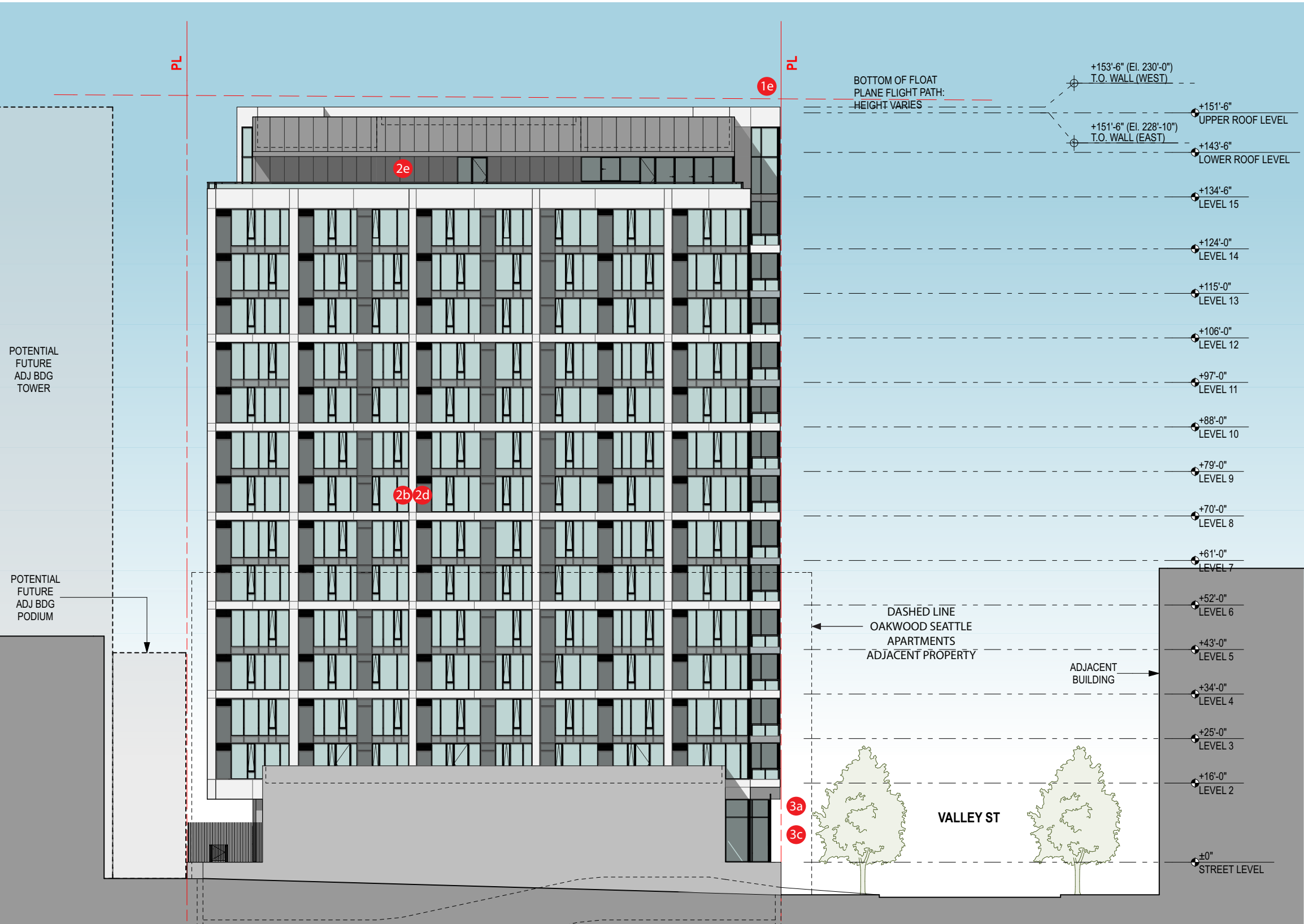
Materials Key:

- Vision Glass
- Spandrel Glass
- Fiber Cement Panel 1
- Fiber Cement Panel 2
- Metal Panel (Not Shown)
- Metal Louvre (Not Shown)
- Aluminum Window Wall System
- Concrete

AERIAL VIEW FROM SOUTHEAST



EAST ELEVATION (PROPERTY LINE)



MATERIAL PALETTE: NORTH & SOUTH ELEVATIONS

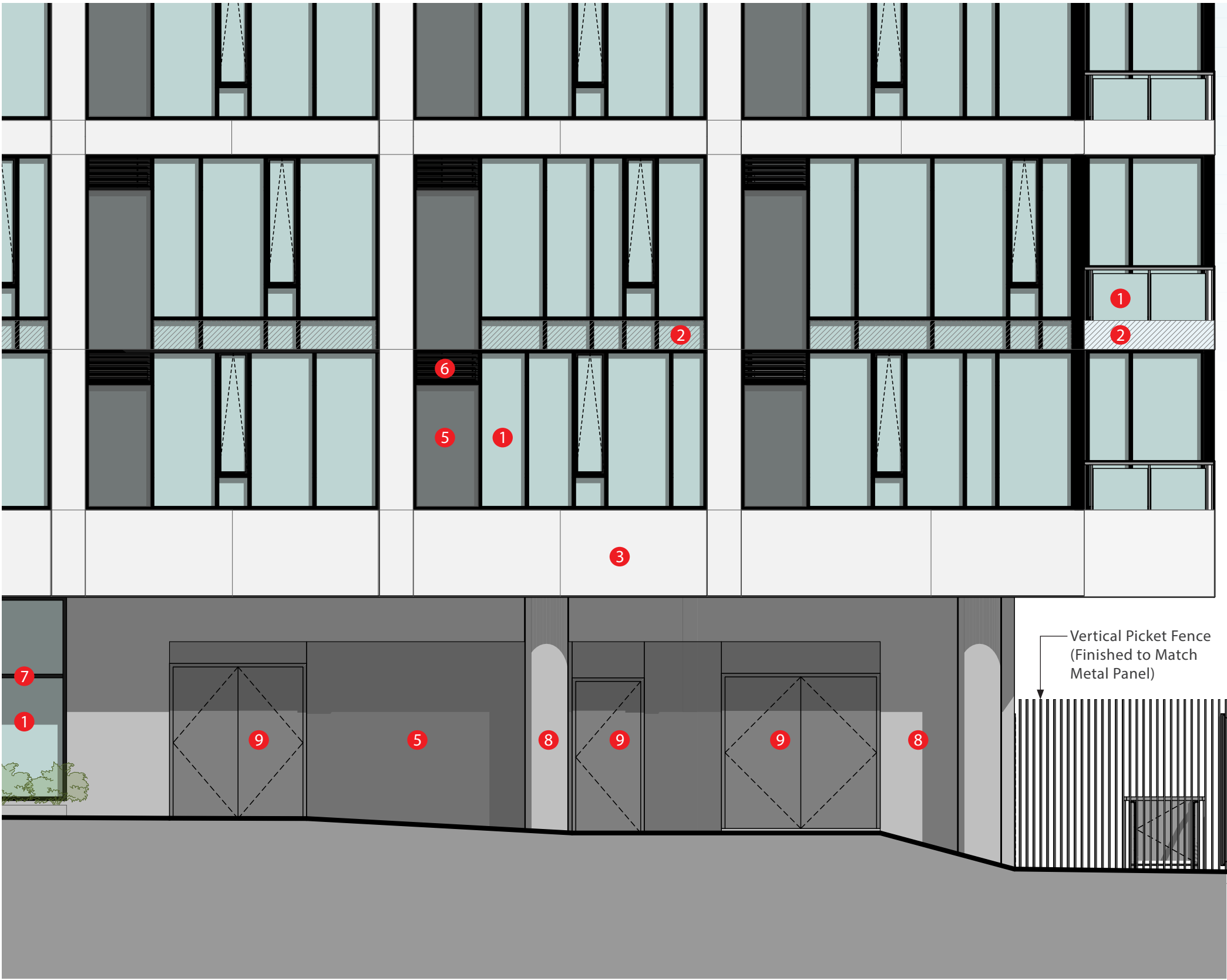


Partial Elevation Detail: Valley St



Partial Elevation Detail: Valley St at Rooftop

MATERIAL PALETTE: EAST & WEST ELEVATIONS



Partial Elevation Detail: Aurora Ave at Service Area

- 1 Low-e Coated Vision Glass
- 2 Spandrel Glass
- 3 Integral Color Fiber Cement Panel 1
- 4 Integral Color Fiber Cement Panel 2
- 5 Metal Panel
- 6 Metal Louver
- 7 Window Wall System
- 8 Concrete
- 9 Hollow Metal Door & Frame
- 10 Open Air Coiling Grille (Not Shown)

LEVEL 2 TERRACE & TYPICAL FLOOR PLANS

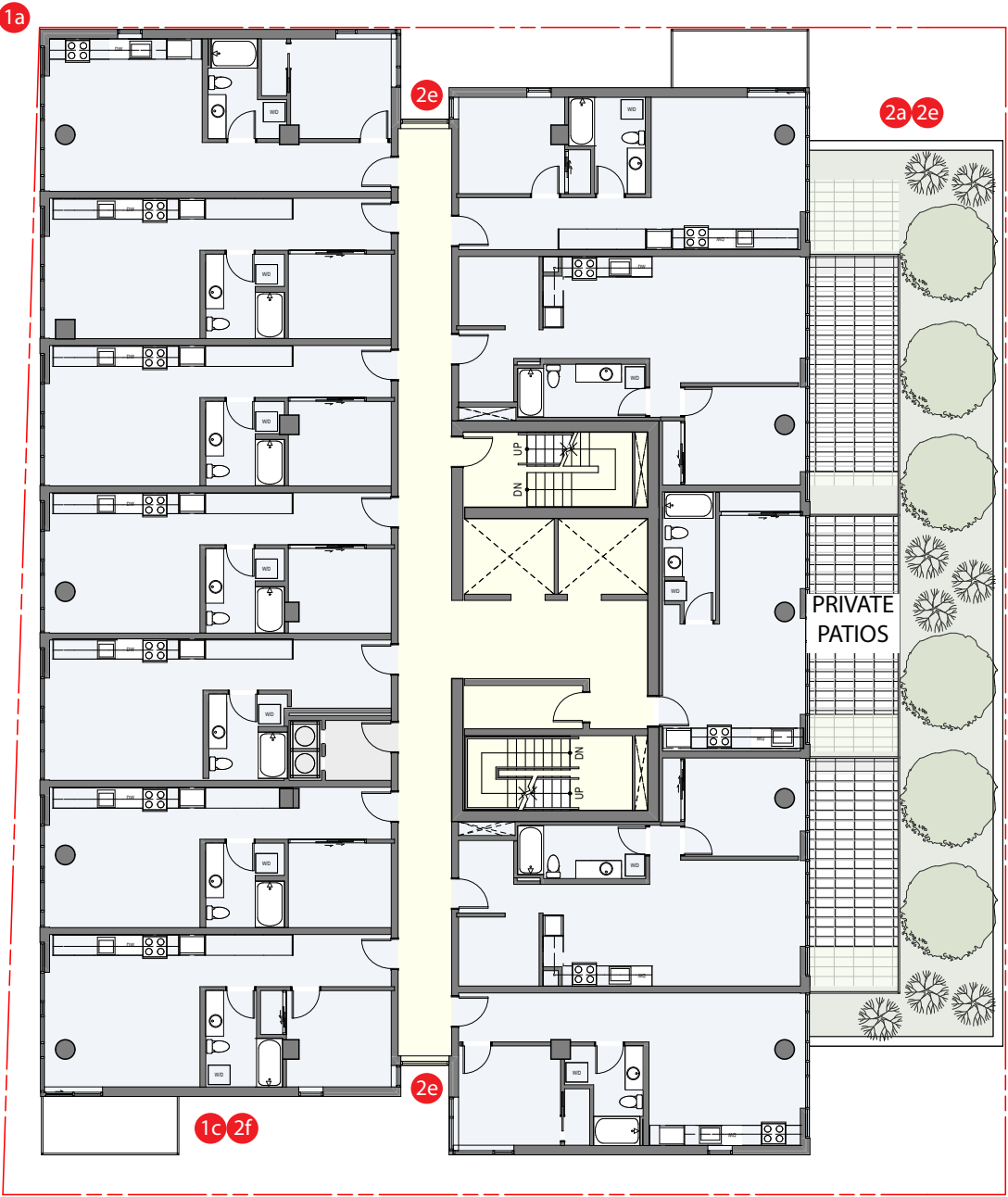
- CS A2 Architectural Presence
- CS B1 Site Characteristics
- CS D5 Respect for Adjacent Sites
- DC A1 Site Characteristics and Uses
- DC A2 Reducing Perceived Massing

Massing

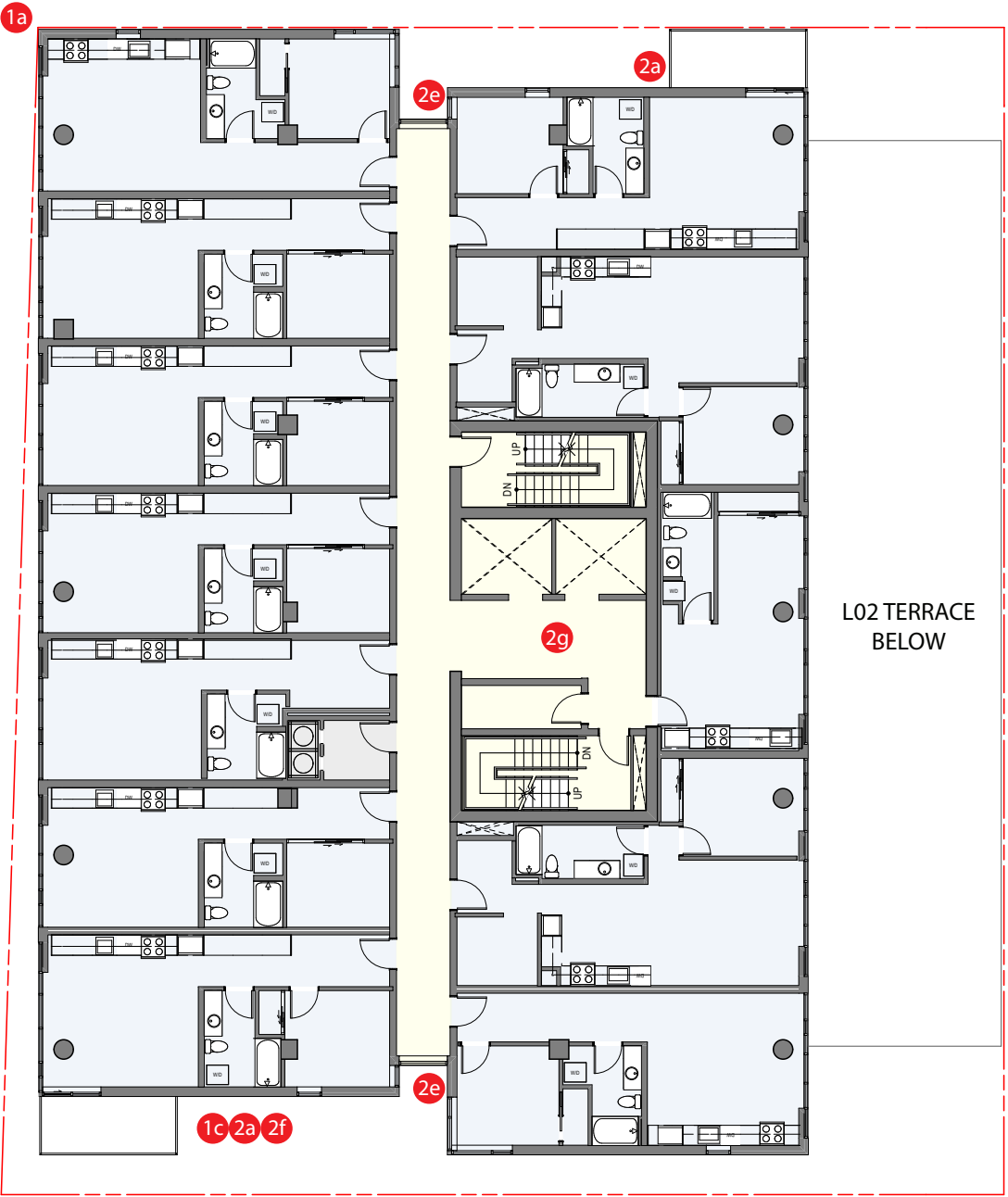
- 1a Tower is positioned at the NW corner of the site
- 1c A reduced footprint creates a uniform, slender tower

Tower Design

- 2a Concept expressed through masses that shift in plan and step in elevation
- 2e Common spaces are recessed, contiguous, and expressed with a complimentary materiality
- 2f Massing of the tower creates two slightly unique expressions envisioned as part of a whole



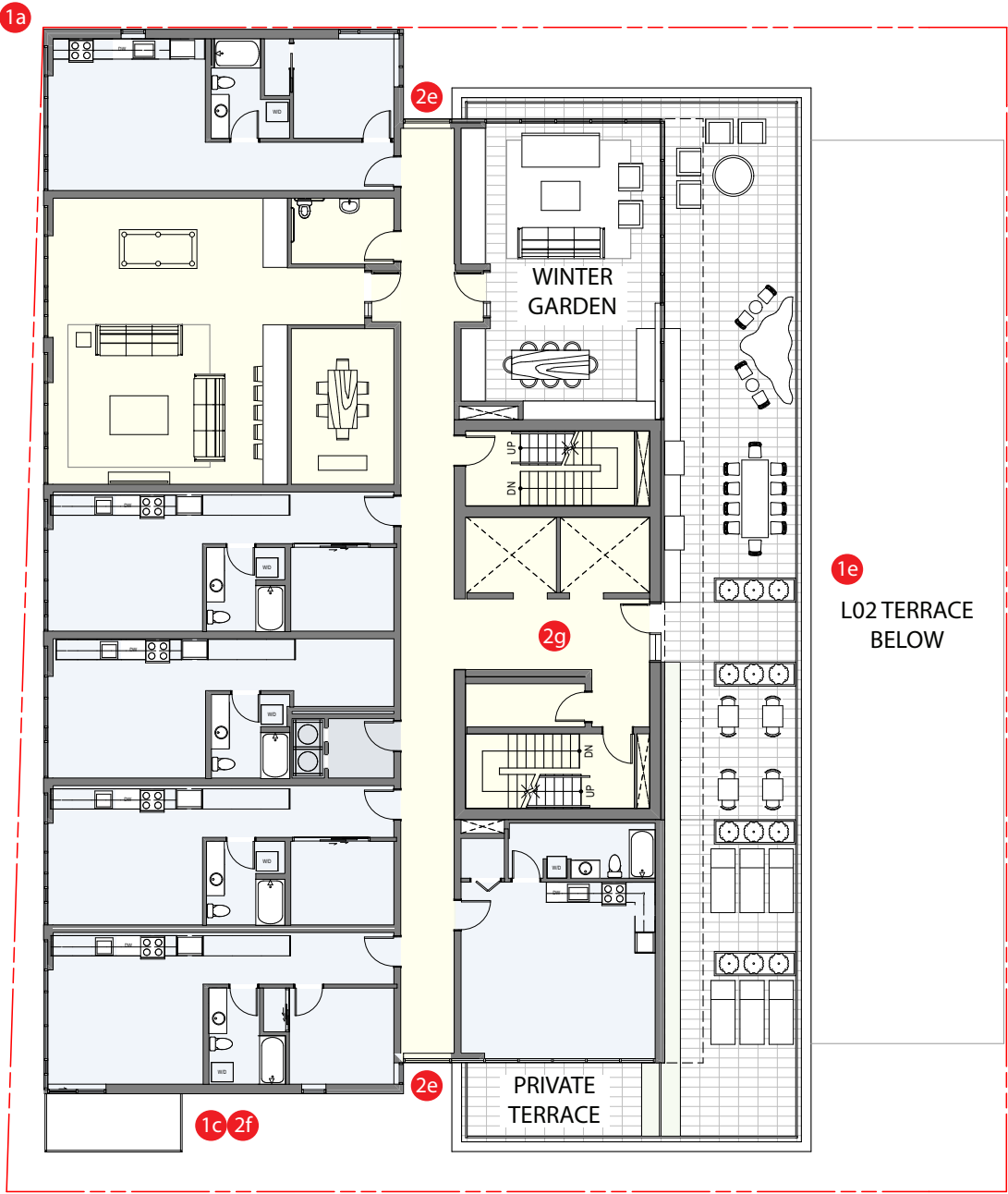
Second Floor & Terrace Plan



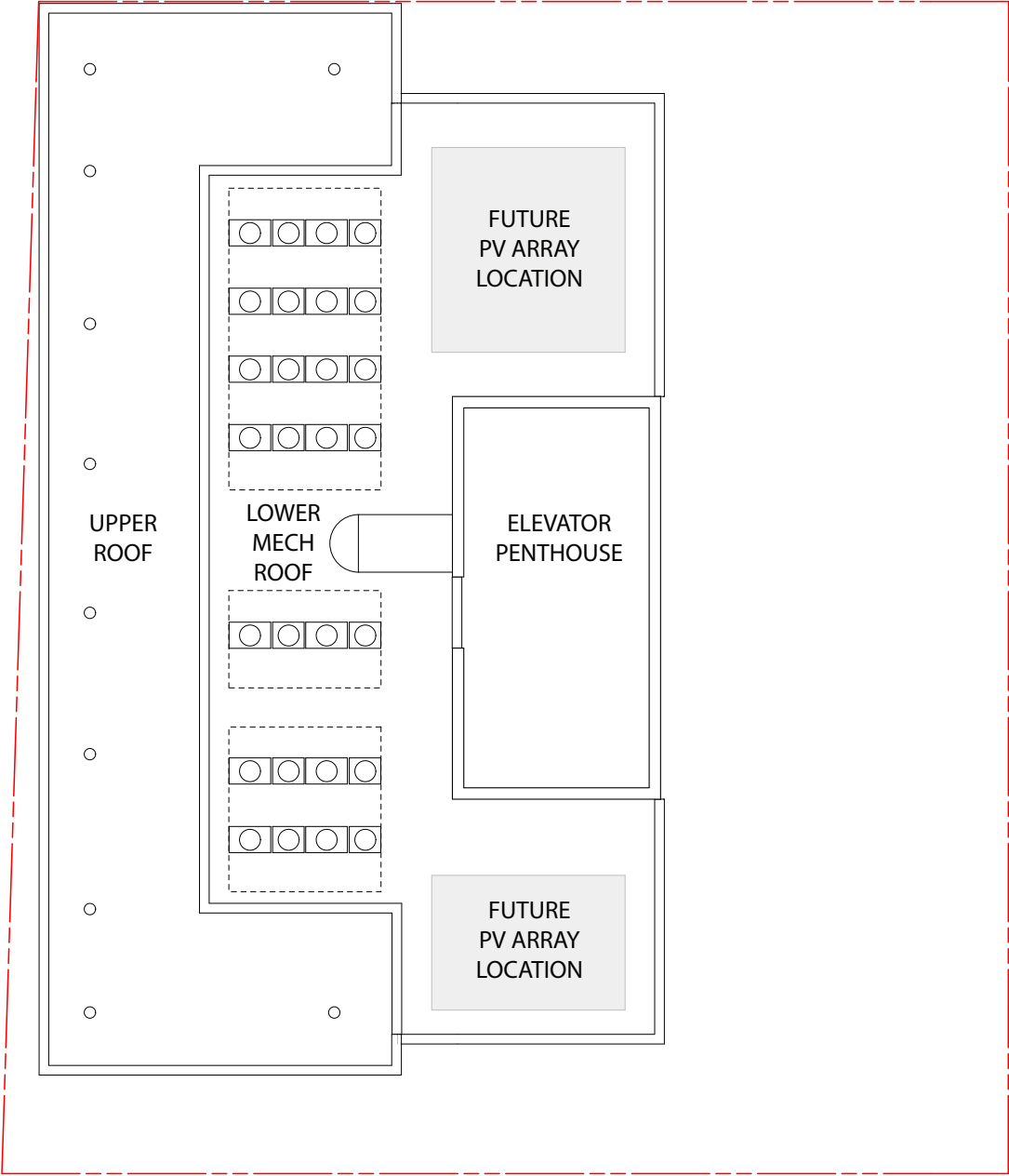
Typical Floor Plan

L02 TERRACE
BELOW

LEVEL 15 TERRACE & ROOF PLANS



Level 15 Floor & Terrace Plan



Roof Plan

- CS A2 Architectural Presence
- CS B1 Site Characteristics
- CS D5 Respect for Adjacent Sites
- DC A1 Site Characteristics and Uses
- DC A2 Reducing Perceived Massing

Massing

- 1a Tower is positioned at the NW corner of the site
 - 1c A reduced footprint creates a uniform, slender tower
- ### Tower Design
- 2a Concept expressed through masses that shift in plan and step in elevation
 - 2e Common spaces are recessed, contiguous, and expressed with a complimentary materiality
 - 2f Massing of the tower creates two slightly unique expressions envisioned as part of a whole
 - 2g An asymmetrical building core creates different unit layouts on each half

BUILDING SECTION FACING NORTH

- CS2 A2 Architectural Presence
- CS2 B1 Site Characteristics
- CS2 D5 Respect for Adjacent Sites
- DC2 A1 Site Characteristics and Uses
- DC2 A2 Reducing Perceived Massing

Massing

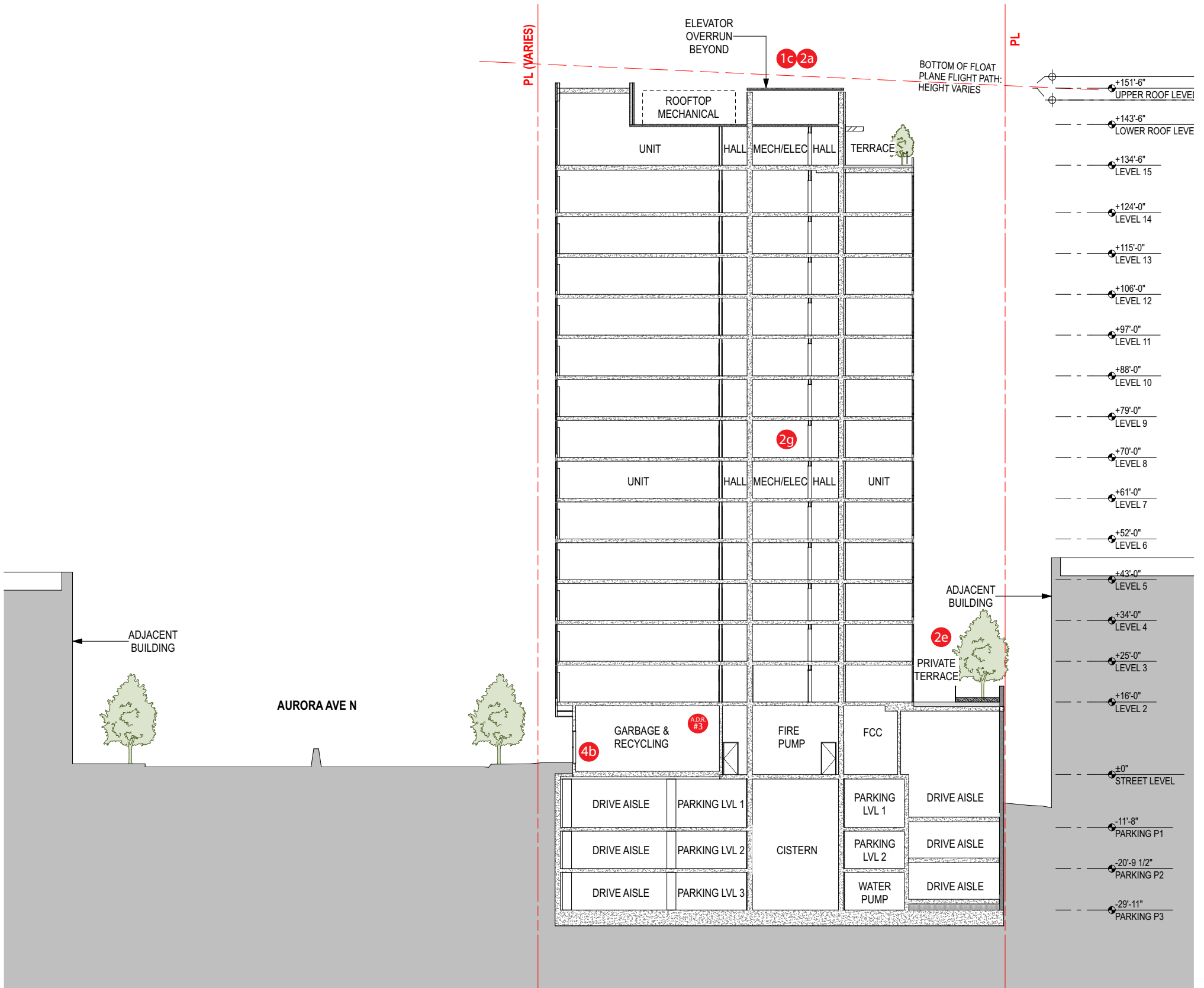
- 1c** A reduced footprint creates a uniform, slender tower

Tower Design

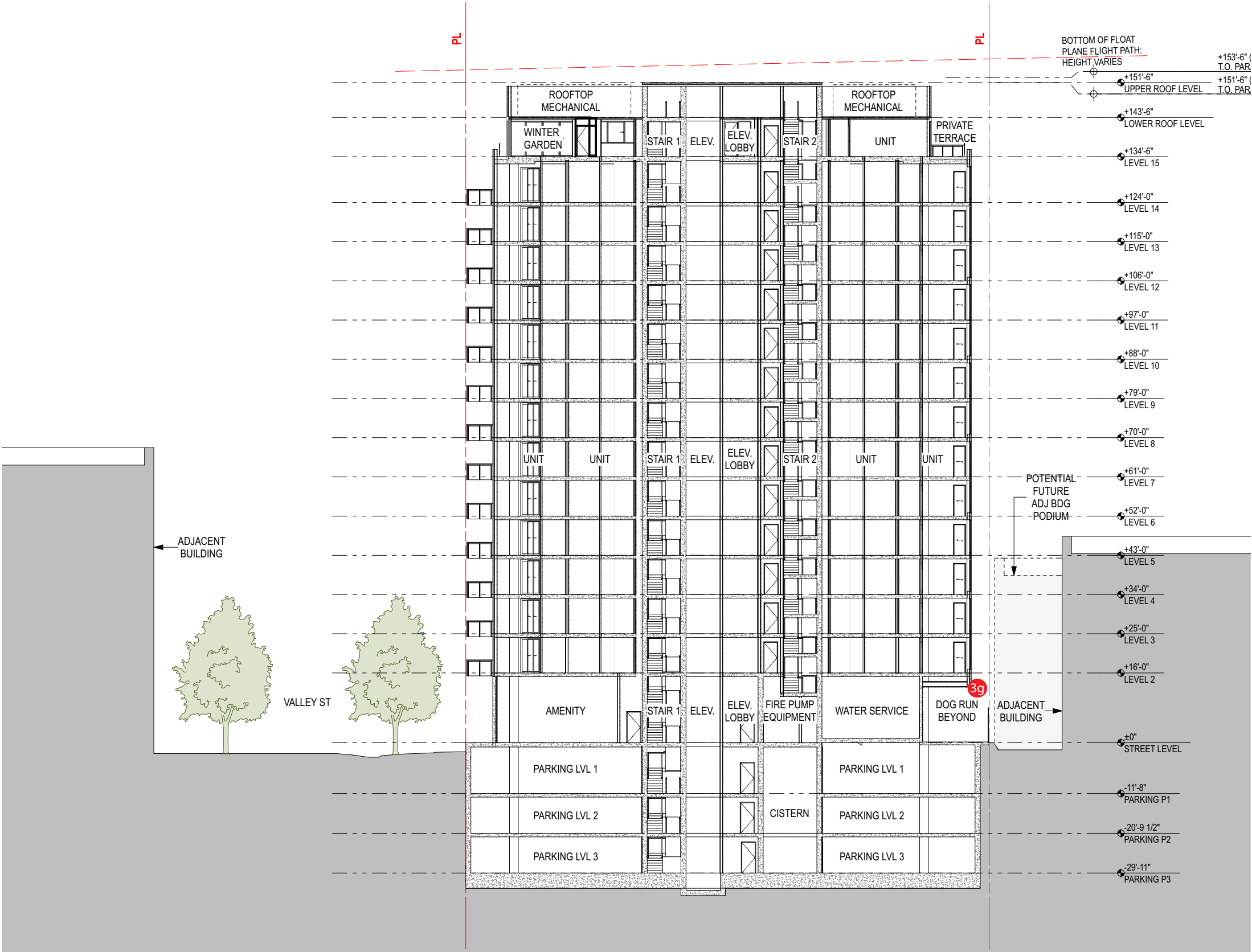
- 2a Concept expressed through masses that shift in plan and step in elevation
- 2e Common spaces are recessed, contiguous, and expressed with a complimentary materiality
- 2g An asymmetrical building core creates different unit layouts on each half

Access

- 4b** Solid waste pickup utilizes the existing curb cut on Aurora



BUILDING SECTION FACING EAST

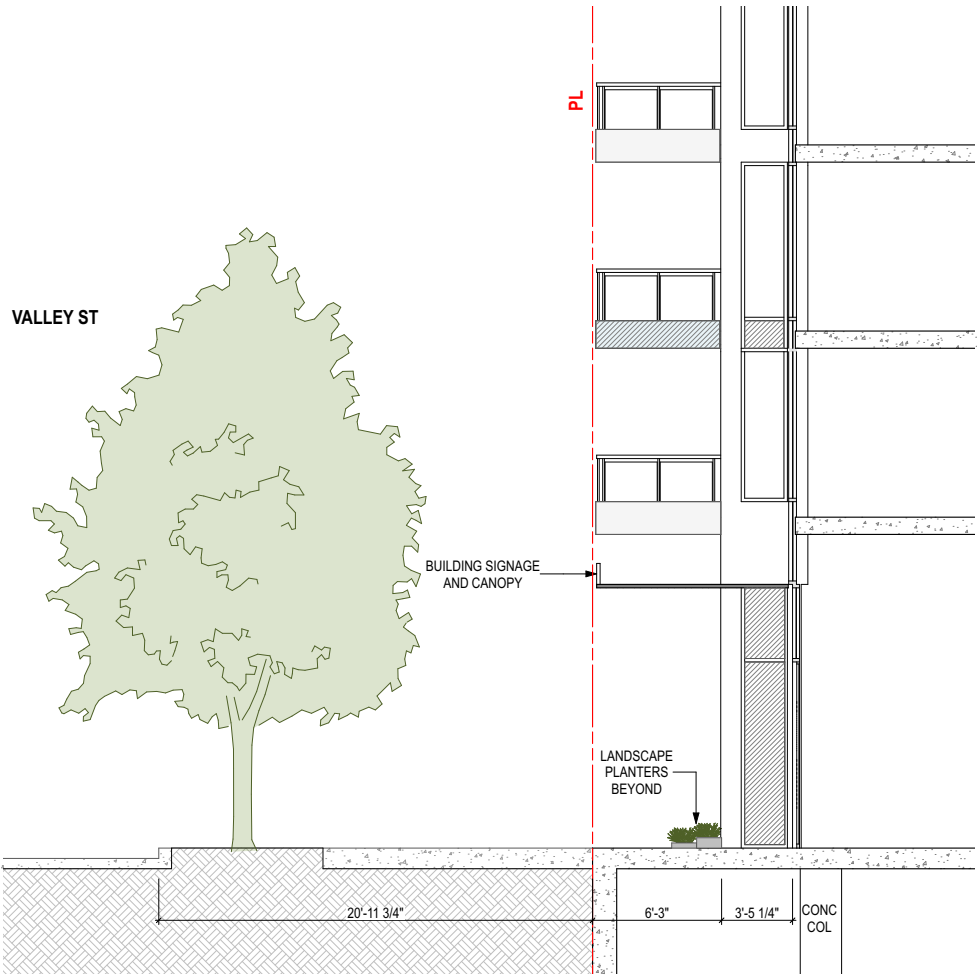


- PLD A2 Access Challenges
- PLD A1 Serving All Modes of Travel
- PLD A2 Connections to All Modes
- DCI A1 Visibility
- DCI B1 Access Location and Design

Relationship to Street

- 3g Access to the rear courtyard is through a secure gate

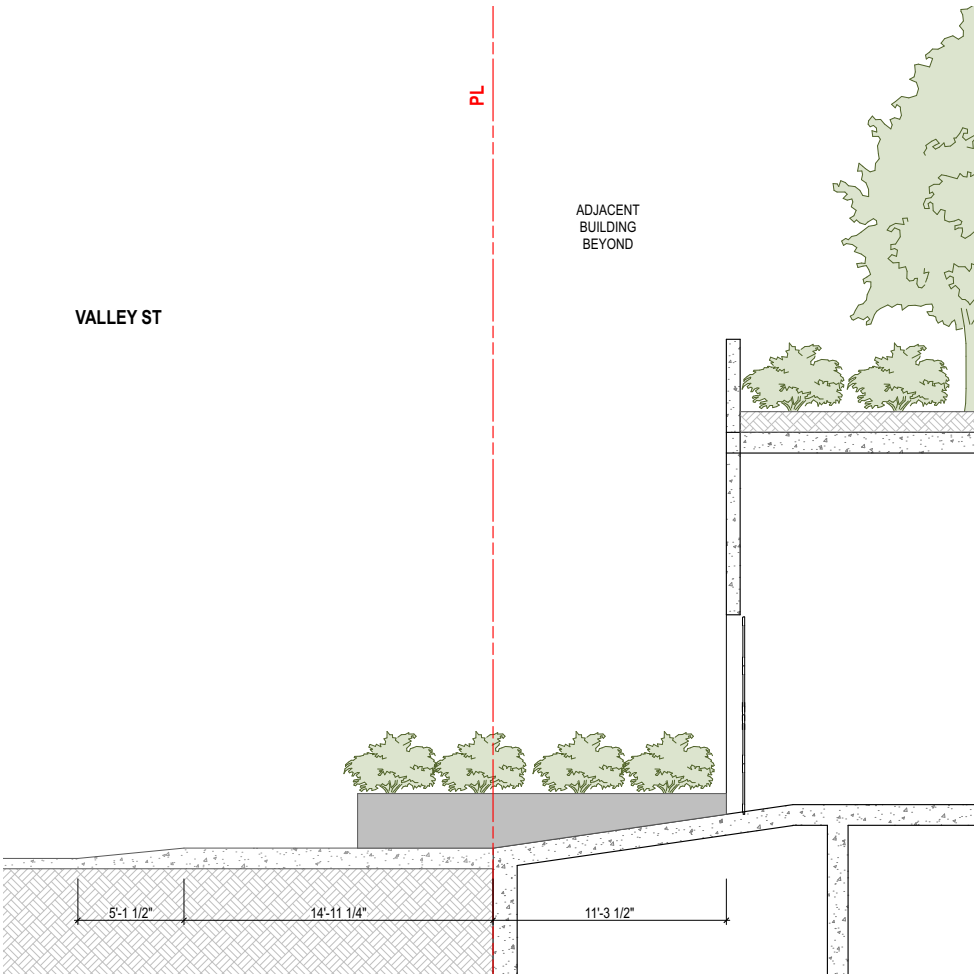
ENLARGED EXTERIOR STREET LEVEL SECTIONS



1 Main Entry off Valley St

a. The Board encouraged the location of the entry lobby and the cascading stairs towards Dexter Ave N. (CS2.B.2, PL2.A.2, PL3.A.1&2&4)

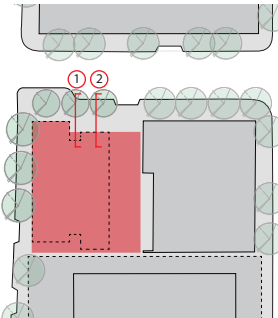
Design Response: The primary pedestrian entry is located on Valley Street, clearly marked by the building's massing. Given that most pedestrians arriving to the side will be coming uphill from Dexter Avenue to the east, this lobby entrance is both convenient and visible. The stepped landscaped planters also serve to guide the pedestrians to the entrance.

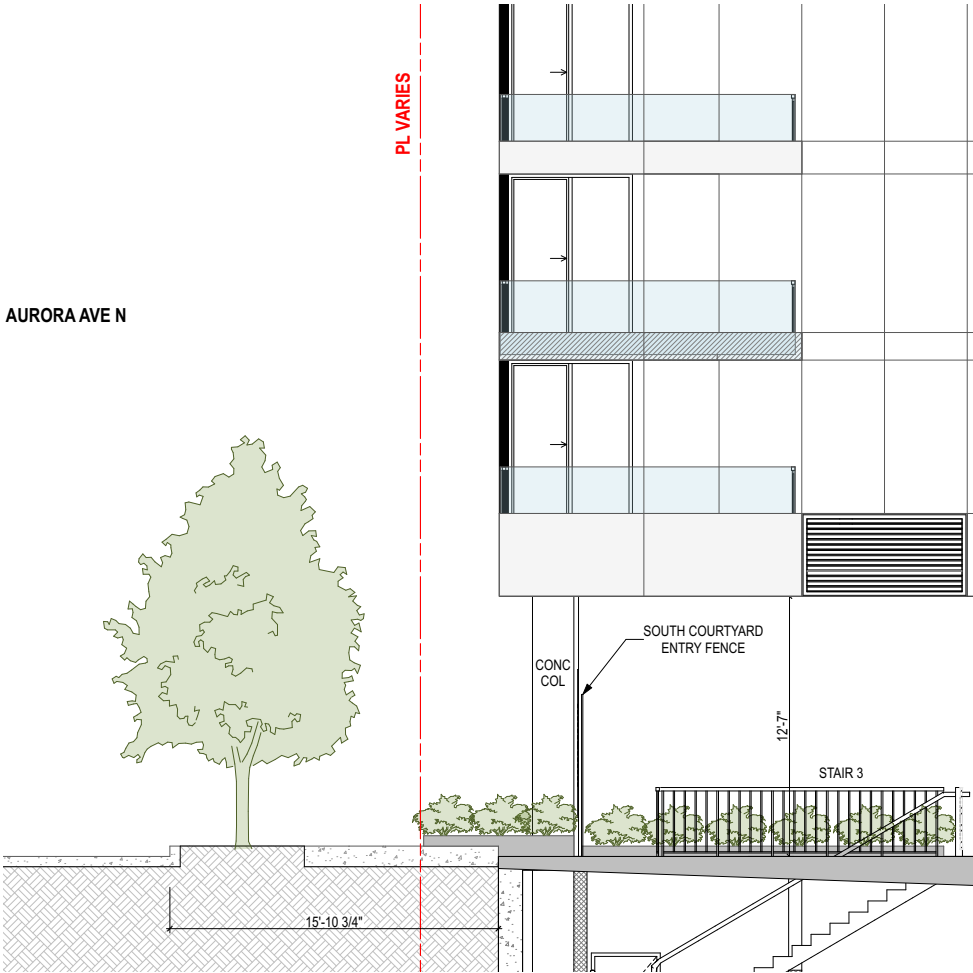


2 Garage Entry off Valley St

b. Design the street level for residents to enter and exit the site safely. (PL2.A.2, PL2.B.3, PL3.A.1&2, PL4.A.2, DC1.B.1)

Design Response: The main pedestrian entry is located on Valley street to provide a safe entry. Conversely, the noise and traffic associated with Aurora Avenue would have made it or the corner an unsuitable location for a pedestrian entry. Locating the SCL vault and Solid Waste access on Aurora helps to separate pedestrian access from these uses. The entry to the parking garage is also located on Valley St, but the curbs, planting, and pavement texture are designed to enhance the pedestrian experience, and increase the awareness that cars are approaching a pedestrian trafficked area.

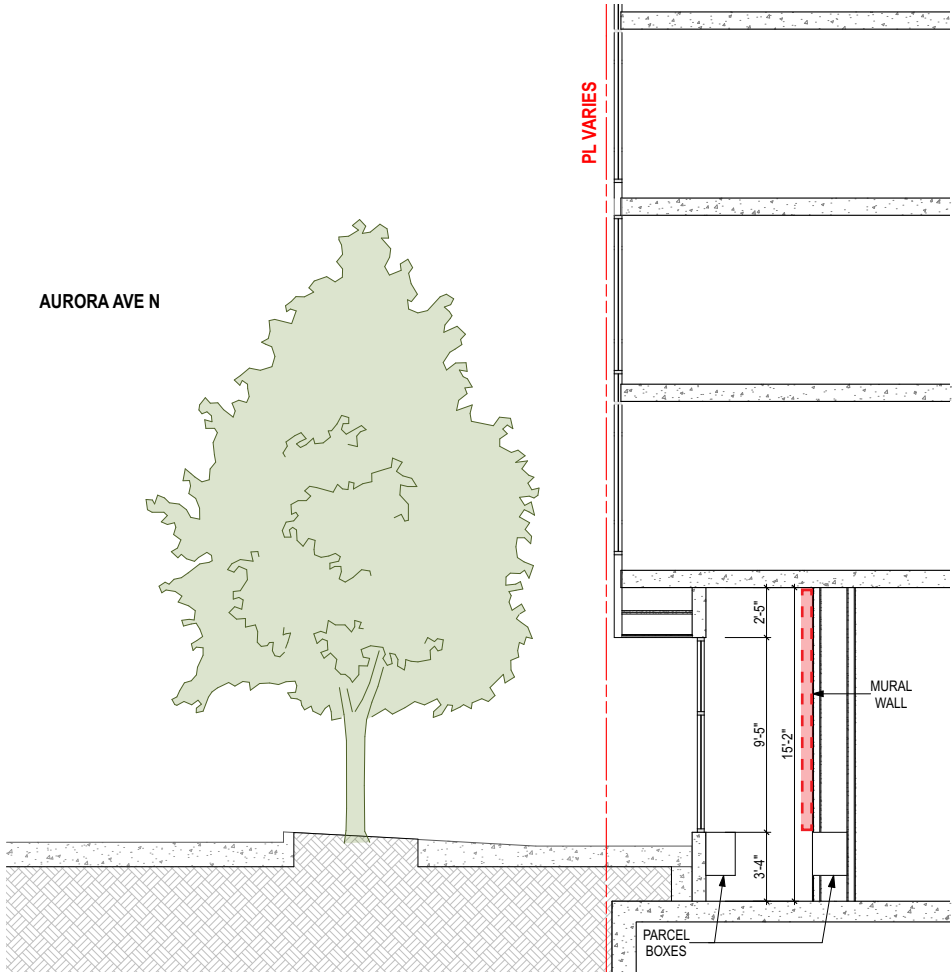




3 South Courtyard off Aurora Ave N

c. Supported the placement of bike storage use along Aurora Ave N. (PL4.A.1, DC1.A.4)

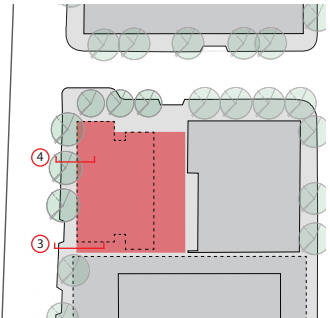
Design Response: The site contains a secondary pedestrian access gate at the southwest corner of the site along Aurora. This gate connects to the rear courtyard, which provides access to the bicycle room, dog run and garage stairs. Resident bicyclists or dog walkers arriving from the south may elect to come up Roy Street, which has a more gradual grade than Valley Street.



4 Residential Lobby Along Aurora

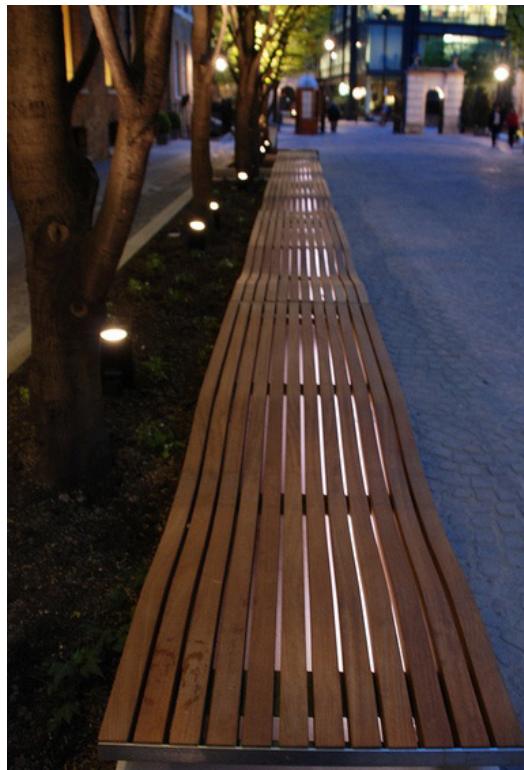
d. Carefully consider how the street edge will work. (DC2.A.1, DC2.B.1&2)

Design Response: As a welcoming gesture to pedestrians, the sidewalk planters step up the steep hill while shifting back towards the building and its entrance. The western residential slab has been shifted up on columns, revealing the common areas of the lobby level as a continuous horizontal gasket open to the public, pedestrian realm.



PLANTING AND PAVING

HARDSCAPE INSPIRATION IMAGES



PLANTING INSPIRATION IMAGES



viburnum davidii



japanese snowbell



sword fern



red tips box honeysuckle



scarlet oak



siberian iris



sky pencil holly



hoogendorn holly



siberica dogwood



ice dance sedge



karl foerster feather reed grass



princess diana serviceberry



big blue liriopse



rocket ligularia



wulfenii spurge



beach strawberry



yellow twig dogwood



orange sedge



stella d'oro daylily

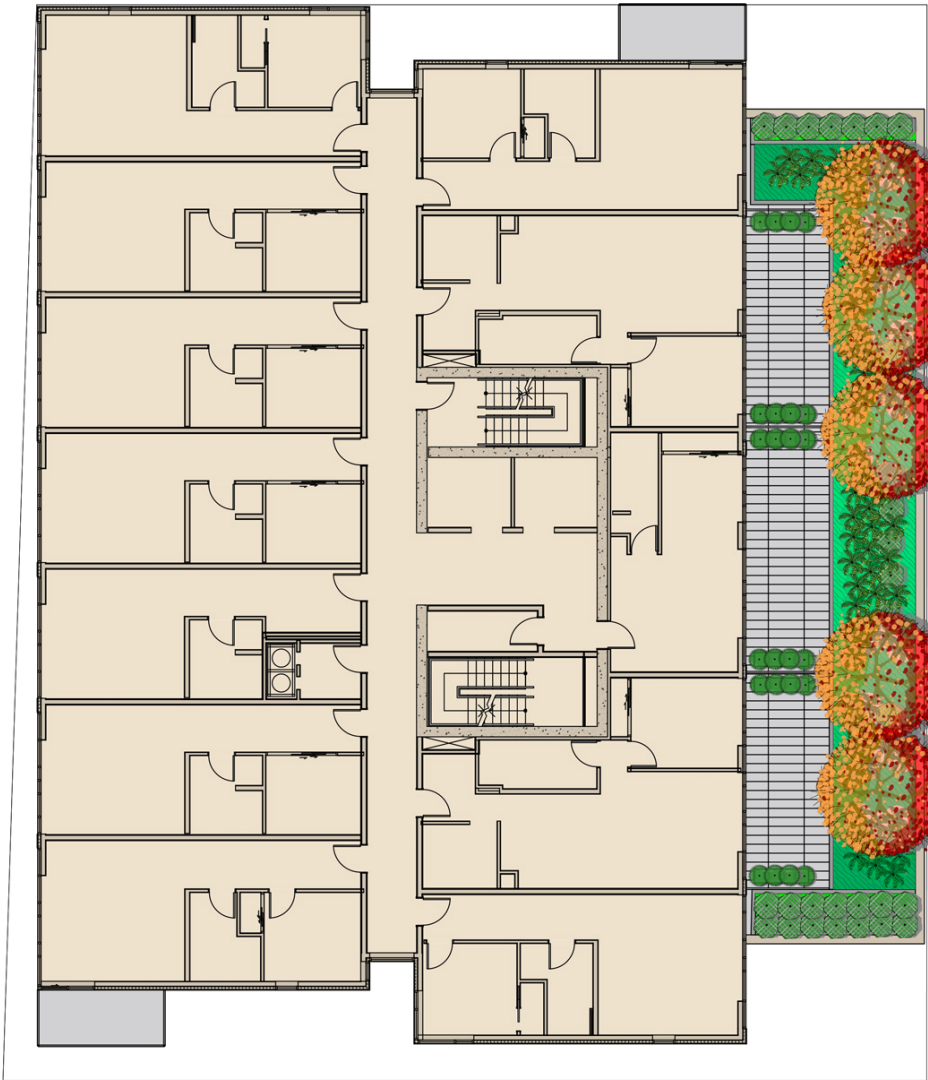


big daddy plantain

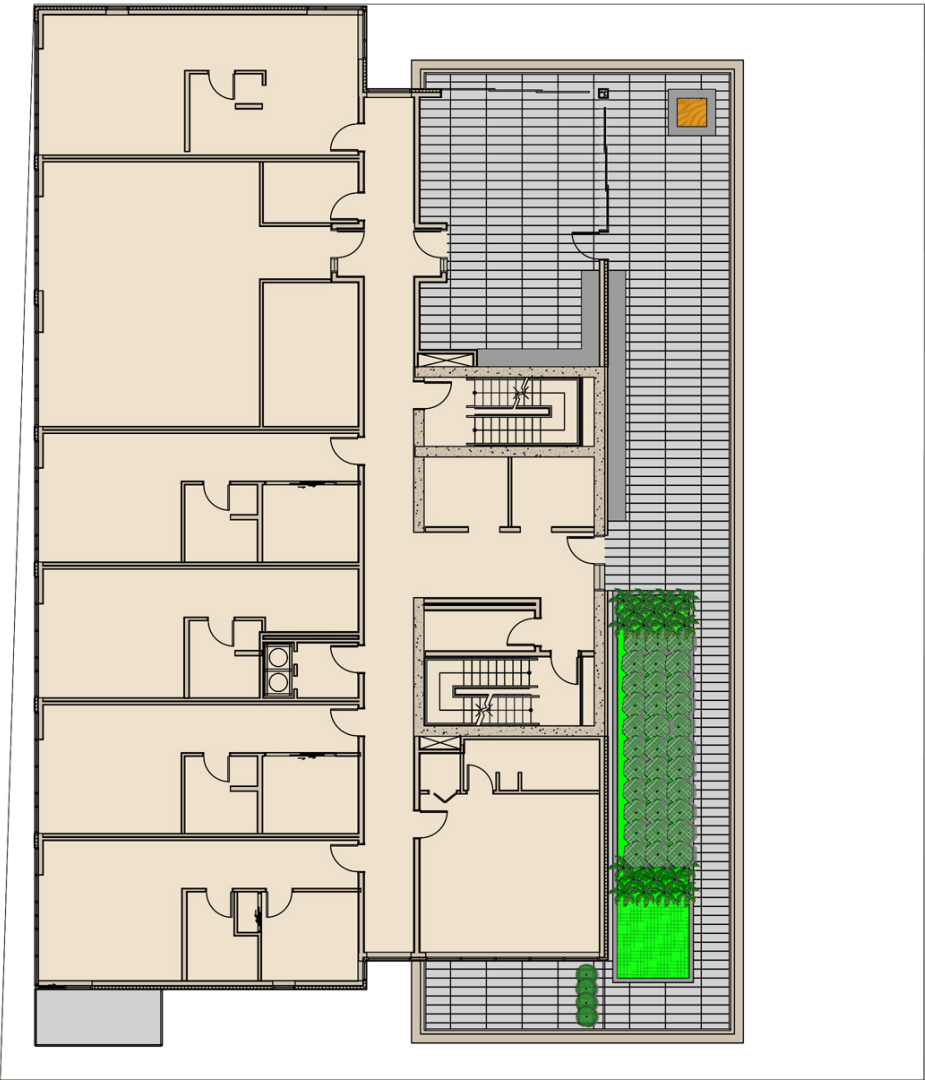
STREET LEVEL LANDSCAPE PLAN



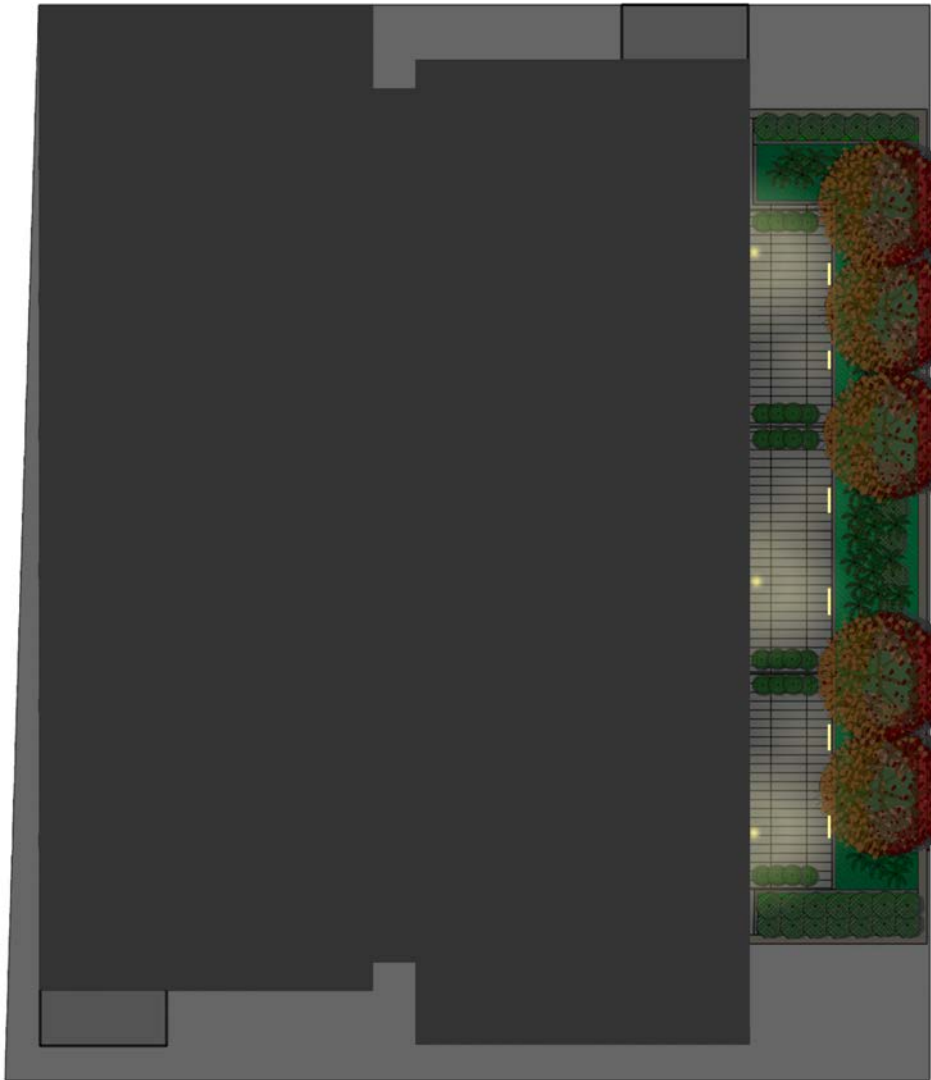
PODIUM & ROOF TERRRACE LANDSCAPE PLANS



Second Floor & Terrace Plan



Roof Terrace Plan



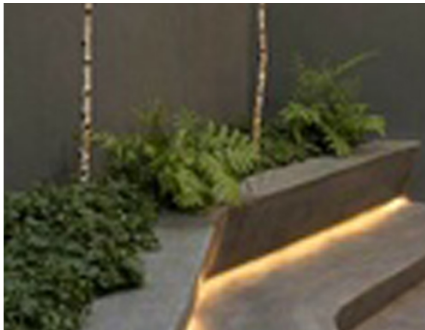
Second Floor & Terrace Lighting Plan



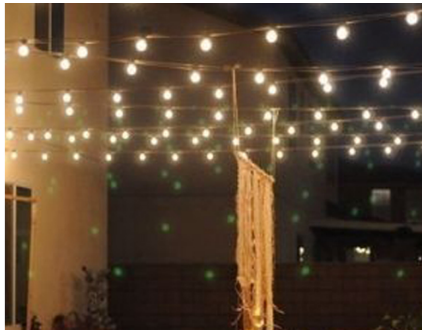
Roof Terrace Lighting Plan

STREET LEVEL EXTERIOR LIGHTING PLAN

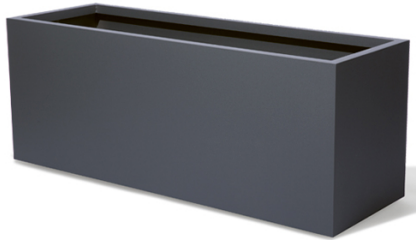
LIGHTING FIXTURE EXAMPLES



strip light in cast planter walls



suspended festival lights



manufactured planter



manufactured bench



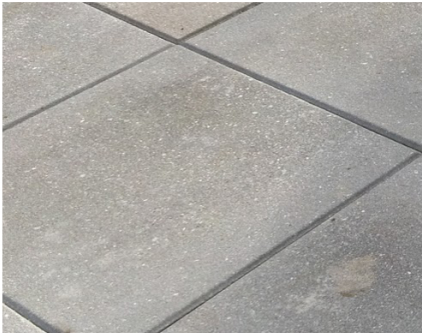
bicycle rack



bicycle rack detail



accent paving with ground glass



precast pedestal pavers





Residential Identity Signage

- Wall-mounted
- Integrated with building panel sizes & locations
- Potential for internal or back lighting



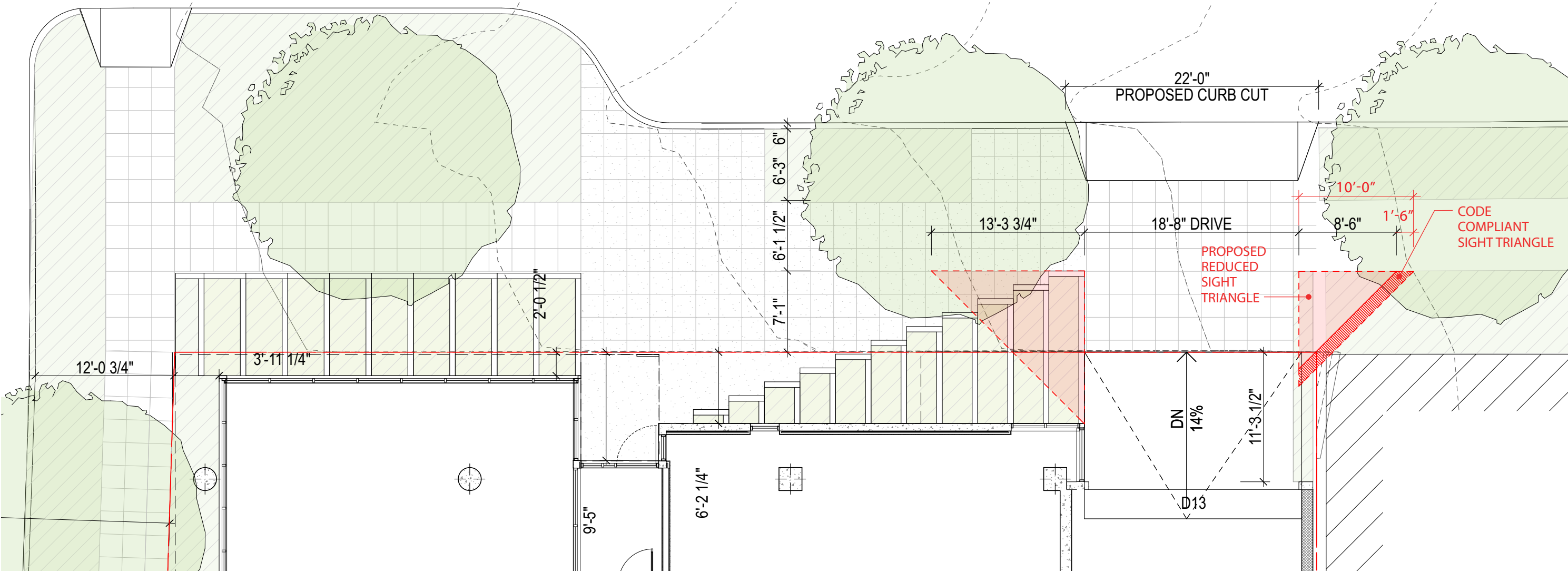
Residential Address Signage

- Integrated with minimalist steel plate entry canopy
- Pin-mounted lettering

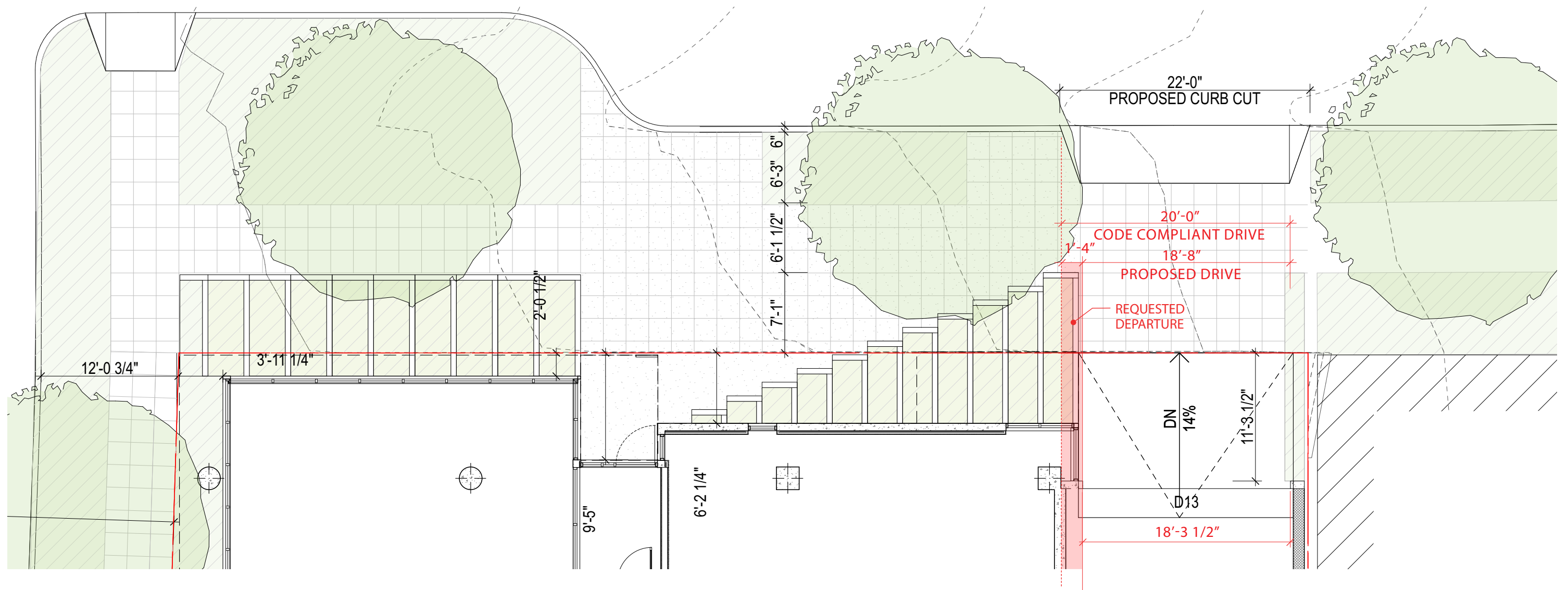


REQUESTED DEPARTURES

DESIGN STANDARD	DEVELOPMENT STANDARD DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE
<p>1 SMC 23.54.030.G.2 Sight Triangle Requirements</p> <p>A ten-foot sight triangle is required at both sides of two-way driveways up to 22-feet wide. When a driveway is located adjacent to a property line, the sight triangle can be achieved by offsetting the driveway or by procuring an easement from the neighbor.</p>	<p>Reduce required sight triangle by 1'-6" to allow an 8'-6" sight triangle at the exit side of the driveway and use textured pavement and at-grade warning lights to address pedestrian safety</p>	<p>Reducing the sight triangle requirement will help to minimize the size of the parking garage entry and lessen its appearance from the sidewalk. This is especially important for the project, as Valley Street will be the primary approach and entry to the building. To achieve a safe resulting environment, the garage entry is set back from the property line about 11'-6" and the sidewalk is located almost 8' beyond the property line resulting in nearly 20' from the garage door to sidewalk. The gradient of the parking ramp is minimized at this location to provide an opportunity for vehicles to pause prior to entering the sidewalk. The use of textured paving is an effective way of subtly marking intersections at the sidewalk and is a method used by other multi-family projects located in South Lake Union. At-grade, in-slab warning lights are an additional unobtrusive way to signal to pedestrians that a vehicle is approaching, and will supplement the use of textured paving and address low-light level situations. (Design Guidelines CS2-B.2 Connection to Street, DC1-C.1 Below-Grade Parking, DC1-C.2 Visual Impacts)</p>

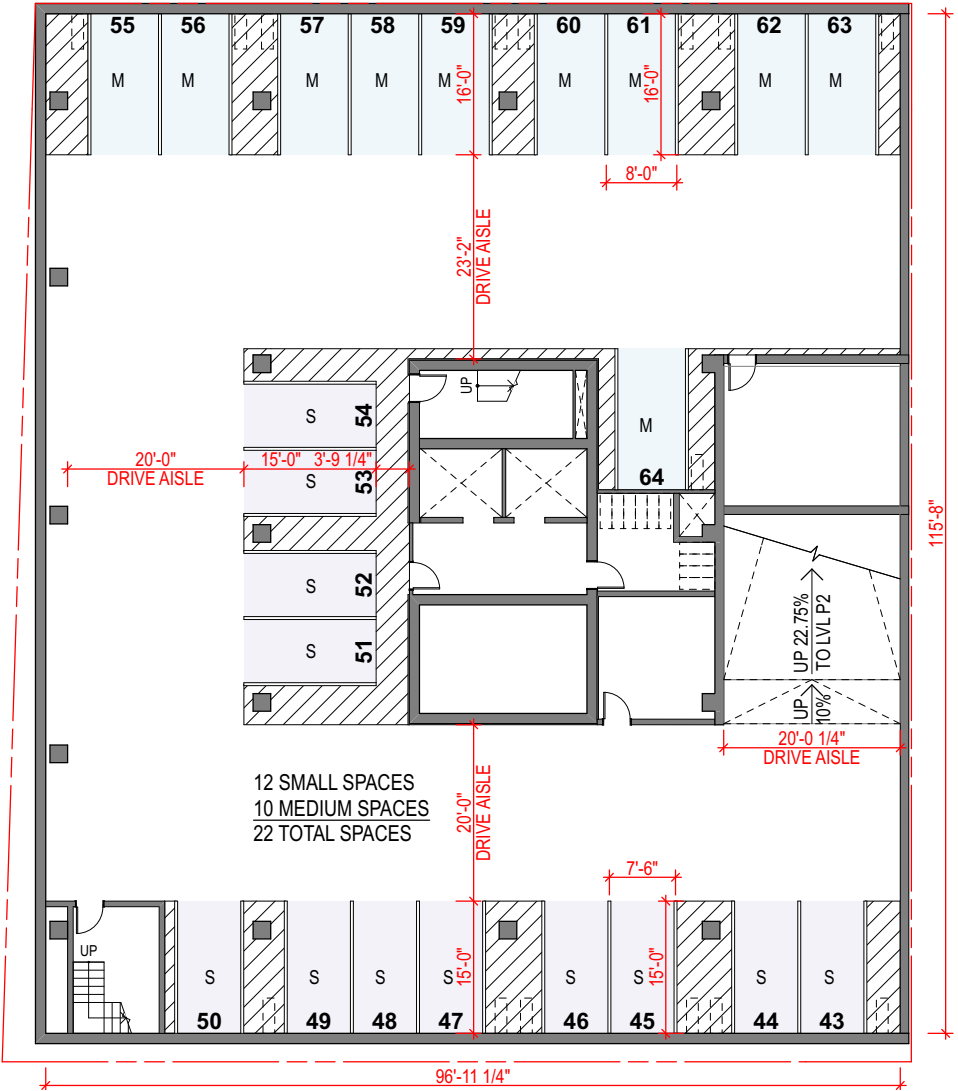
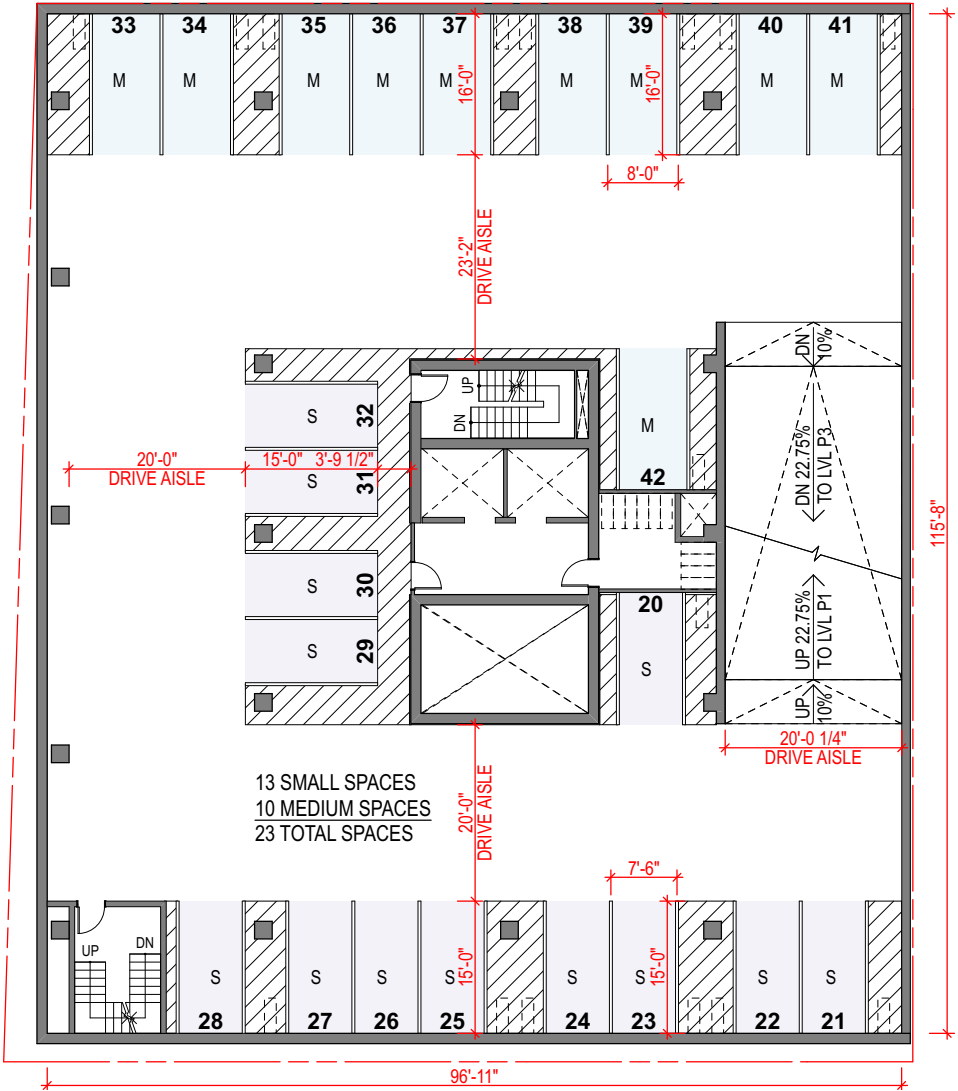
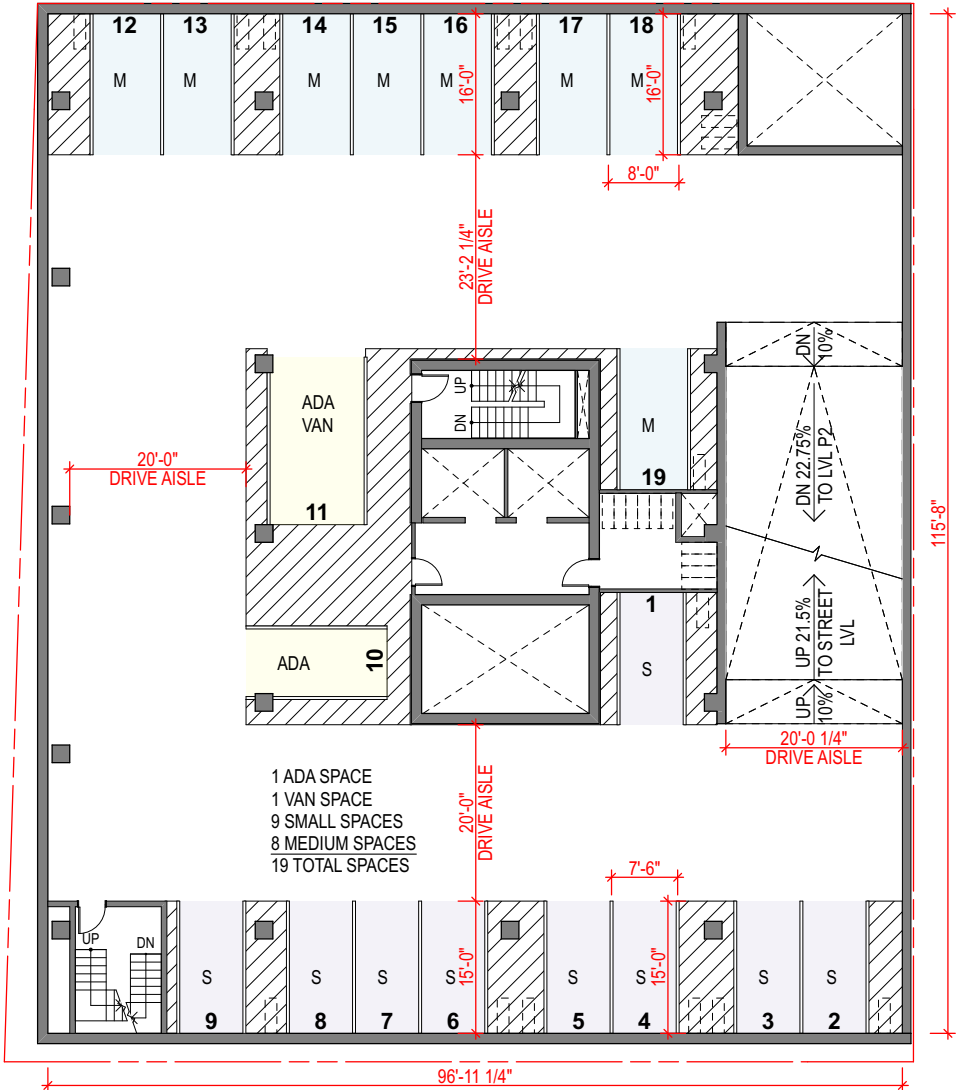


DESIGN STANDARD	DEVELOPMENT STANDARD DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE
2 SMC 23.54.030.D.1.c Driveways Driveways of any length that serve more than 30 parking spaces shall be at least 10 feet wide for one-way traffic and at least 20 feet wide for two-way traffic.	Reduce required driveway width by 1'-4" to allow an 18'-8" driveway at the parking garage entry of Valley St.	Allowing a narrower driveway width on the entrance side of the garage entry will help to diminish the presence of the garage entry and emphasize the pedestrian realm along Valley Street. As noted for Departure Request #2, the emphasis on the pedestrian environment along Valley Street is especially important to the project, as it is the primary approach and entry to the building. (Design Guidelines CS2-B.2 Connection to Street, DC1-C.2 Visual Impacts)



REQUESTED DEPARTURES

DESIGN STANDARD	DEVELOPMENT STANDARD DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE
<p>3 SMC 23.54.030.B.1 Parking Space Mix</p> <p>When more than five parking spaces are provided, a minimum of 60 percent of the parking spaces shall be striped for medium vehicles. The minimum size for a medium parking space shall also be the maximum size. Forty percent of the parking spaces may be striped for any size category in subsection 23.54.030.A, provided that when parking spaces are striped for large vehicles, the minimum required aisle width shall be as shown for medium vehicles.</p>	<p>Reduce required percentage of medium stalls by 25% from 60% to 45%.</p>	<p>Given the tight geometries of the site and the structural requirements of the project, there is not enough room to accommodate the required widths for medium parking stalls and associated drive aisles on more than one side of the parking level. The proposed design of locating the medium stalls on the north side of the parking level, allows for the largest amount of medium stalls possible, approximately 45% of the total number of stalls required. An efficient, yet functional, garage layout with fewer medium stalls allows the project to put all the parking below grade, which maintains the street-level for more pedestrian-oriented design.</p>



Parking Level 1

Parking Level 2

Parking Level 3

